



# Syracuse City Planning Commission Meeting January 15, 2019

Begins at 6:00 p.m. in the City Council Chambers  
1979 West 1900 South, Syracuse, UT 84075

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## Regular Meeting Agenda

### PLANNING COMMISSIONERS

#### CHAIR

Brett Cragun

#### VICE CHAIR

Larry Johnson

Curt McCuiston  
Greg Day  
Dale Rackham  
Grant Thorson  
Ralph Vaughan  
G. Ivy Ruelan

1. **Meeting Called to Order**
  - Invocation or Thought by Commissioner **Day**
  - Pledge of Allegiance by Commissioner **Thorson**
  - Adoption of Meeting Agenda
2. **Meeting Minutes**
  - November 6, 2018 Regular Meeting & Work Session
  - December 4, 2018 Regular Meeting & Work Session
3. **Public Comment**, this is an opportunity to address the Planning Commission regarding your concerns or ideas, regarding items that have not been scheduled for a public hearing on this agenda. Please limit your comments to three minutes.
4. **Public Hearing – Proposed Annexation Petition**, located approx. 3800 W 3000 S
5. **Final Plat – Criddle Farms North 2**, located approx. 4000 W 1200 S
6. **2019 Thought-Pledge Schedule**
7. **Adjourn**

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## Work Session

1. **Department Business**
  - a. City Council Liaison Report
  - b. City Attorney Updates
  - c. Upcoming Agenda Items
2. **Discussion Items**
  - a. IBI Study Presentation
  - b. §10.35.040 Home Occupation
3. **Commissioner Reports**
4. **Adjourn**

### NOTE

If you wish to attend a particular agenda item, please arrive at the beginning of the meeting. In compliance with the Americans Disabilities Act, those needing auxiliary communicative aids and services for this meeting should contact the City Office, at 801-614-9626, at least 48 hours prior to the meeting.

Meetings of the Syracuse Planning Commission may be conducted via electronic means pursuant to Utah Code Ann. §52-4-207. In such circumstance, contact will be established and maintained via electronic means and the meeting will be conducted pursuant to the Rules, Policies and Procedures established by the Governing Body for electronic meetings.

### CERTIFICATE OF POSTING

This agenda was posted on the Syracuse City Hall Notice Boards, the State Public Notice website at <http://www.utah.gov/pmn/index.html>, and the Syracuse City website at <http://www.syracuseut.com>.



# PLANNING COMMISSION AGENDA

January 15, 2019

## **Agenda Item # 2**

## **Meeting Minutes**

**November 6, 2018 Regular Meeting and Work Session**

**December 4, 2018 Regular Meeting and Work Session**

## Minutes of the Syracuse Planning Commission Regular Meeting, November 6, 2018

Minutes of the Regular Meeting of the Syracuse City Planning Commission held on November 6, 2018, at 6:00 p.m., in the Council Chambers, 1979 West 1900 South, Syracuse City, Davis County, Utah.

**Present:**

Commission Members:	Grant Thorson, Chair	
	Brett Cragun	
	Curt McCuistion	
	Larry Johnson, Vice Chair	
	Ralph Vaughan	
	Gretchen Ivy Ruelan	
City Employees:	Noah Steele, Community & Economic Development Director	
	Royce Davies, Planner	
	Heather Davies, Administrative Professional	
	Paul Roberts, City Attorney	
	Brian Bloeman, City Engineer	
	Jo Hamblin, Deputy Fire Chief	
City Council:	Councilwoman Corrine Bolduc	
	Councilman Dave Maughan	
	Councilman Jordan Savage	
Excused:	Dale Rackham	
	Greg Day	
Visitors:	<b>Alan Prince</b>	<b>Jim &amp; Marlene Ruggles</b>
	<b>Mike Wright</b>	<b>Kristie &amp; Gordy Gerszewski</b>
	<b>Sharen Portillo</b>	<b>Nancy Ashby</b>
	<b>Cody Opperman</b>	<b>Ryan Thurgood</b>
	<b>Bennett Thurgood</b>	<b>Christian Jou</b>
	<b>Bonnie &amp; Brent Bourgeois</b>	

**6:03:23 PM**

**1. Meeting Called to Order:**

Chair Thorson called the meeting to order. Commissioner McCuistion provided a thought. The Pledge of Allegiance was led by Commissioner Johnson.

**6:04:42 PM**

COMMISSIONER **MCCUISTION** MADE A MOTION TO ADOPT THE PLANNING COMMISSION AGENDA FOR THE NOVEMBER 6, 2018 MEETING AS PUBLISHED. THE MOTION WAS SECONDED BY COMMISSIONER **JOHNSON**. ALL WERE IN FAVOR, THE MOTION CARRIED UNANIMOUSLY.

**6:05:07 PM**

**2. Meeting Minutes:**

COMMISSIONER **VAUGHAN** MADE A MOTION TO APPROVE THE MEETING MINUTES FOR OCTOBER 2, 2018 REGULAR MEETING AND WORK SESSION AS PUBLISHED. COMMISSIONER **MCCUISTION** SECONDED THE MOTION. ALL WERE IN FAVOR. THE MOTION CARRIED UNANIMOUSLY.

**6:05:54 PM**

**3. Public Comment:** This is an opportunity to address the Planning Commission regarding your concerns or ideas, regarding items that have not been scheduled for a public hearing on this agenda. Please limit your comments to three minutes.

Open public comment.

**6:06:28 PM**

Close public comment.

**6:06:30 PM**

**4. Public Hearing – Rezone Request A-1 TO PRD, located approx. 3000 S Bluff Road**

Mr. Noah Steele, shares that this is an application for a rezone for property located along Bluff Road and approx. 1500 W. The applicant is looking to change the zone from A-1 to PRD, which allows for up to 6 units per acre.

**6:10:07 PM**

Chair Thorson asks the applicant to come forward. Mr. Braxton Guymon, a land use attorney for Monterey Properties, came forward to shares the processes that they have been going through with the City to go forward in their project. Mr. Guymon shares that they are proposing a max of 5 units per acres with a total of 66 units with a development agreement.

**6:16:05 PM**

Open Public Hearing.

64 [6:16:30 PM](#)

65 Mr. Brad Roennebeck, neighbor of the property, shares that he is against the project originally but is happy for the  
66 community. Mr. Roennebeck shares his concerns for the increase in traffic and access on to Bluff. Mr. Roennebeck is against  
67 the increase in density.

68 [6:18:32 PM](#)

69 Ms. Bonnie Bourgeois, of Syracuse, is curious to what the project will entail. Mr. Paul Roberts, City Attorney, shares  
70 that they are looking at the zoning and whether or not it would be appropriate for this area. Ms. Bourgeois is not in favor of the  
71 increased density and the increase of traffic along Bluff and Jensen Pond.

72 [6:21:16 PM](#)

73 Close Public Hearing.

74 [6:21:32 PM](#)

75 Mr. Guymon, the applicant's representative, shares that the neighboring subdivision is the same density as the  
76 proposed project. Mr. Guymon shares that there is less traffic with a 55+ community.

77 [6:24:10 PM](#)

78 Mr. Vaughan shares that this area will be impacted greatly by the WDC coming in shortly. Mr. Vaughan shares his  
79 concerns for approving this and having PRD all along the west side of Bluff Rd.

80 [6:31:04 PM](#)

81 Chair Thorson shares Mr. Vaughan's concerns for the accesses on Bluff Rd.

82 [6:32:17 PM](#)

83 COMMISSIONER **VAUGHAN** MOVED THE PLANNING COMMISSION RECOMMEND THAT THE CITY COUNCIL  
84 APPROVE THE REQUEST OF MONTEREY PROPERTIES LLC TO AMEND THE CURRENT ZONING MAP FROM A-1 TO  
85 PRD FOR THE 13.527 ACRES LOCATED APPROX 3500 S BLUFF ROAD. COMMISSIONER **CRAGUN** SECONDED THE  
86 MOTION. THIS MOTION PASSES WITH A 5:1 VOTE.

87 [6:33:09 PM](#)

88 **5. Public Hearing – Proposed Annexation by CW Land/LDS Church, located approx. 2600 W 3000 S**

89 Mr. Davies shares information concerning the location of the proposed annexation petition. This annexation will allow for a  
90 connection from 2400 W to Gentile.

91 [6:35:13 PM](#)

92 Commissioner Vaughan has questions if the petition is officially accepted by the City or not. Mr. Roberts shares some  
93 clarifying information. Chair Thorson asks a clarification to what the zone is upon annexation.

94 [6:38:27 PM](#)

95 Ed Graham, CW Land, shares that the idea is to annex this land in order to serve a future community development. Mr.  
96 Vaughan asks what the applicant would prefer for the zoning on the property.

97 [6:40:34 PM](#)

98 Open Public Hearing.

99 [6:40:56 PM](#)

100 Mr. Brent Bourgeois, of Syracuse, shares concerns for bringing in a development in this area that would increase the  
101 traffic and strain the infrastructure.

102 [6:42:49 PM](#)

103 Close Public Hearing.

104 [6:43:06 PM](#)

105 Mr. Graham shares that this is purely a request to annex the land into Syracuse City.

106 [6:43:56 PM](#)

107 Mr. Vaughan shares concerns for the increase effects on the water.

108 [6:48:44 PM](#)

109 Mr. Bloeman, City Engineer, shares that a developer will be required to have water shares for the secondary water.  
110 The City's Water Master Plan did take this land in to account for the increased water for both culinary and secondary water.

111 [6:49:57 PM](#)

112 COMMISSIONER **JOHNSON** MOVED THE PLANNING COMMISSION RECOMMEND APPROVAL OF THE  
113 ANNEXATION OF THIS PROPERTY AND ZONE ON THE GENERAL PLAN MAP UNDER THE R-1 ZONE. COMMISSIONER  
114 **VAUGHAN** SECONDED THE MOTION. THIS MOTION PASSES UNANIMOUSLY.

115 [6:50:36 PM](#)

116 **6. Public Hearing – 2<sup>nd</sup> Amendment to Ninigret North I Lot 1B, located approx. 1100 W 350 S**

117 Mr. Davies shares that the applicant is looking to divide the parcel into 3 lots to allow the buildings to each be on their own  
118 individual parcels.

119 [6:53:00 PM](#)

120 Mr. Michael Wright, of Antelope Business Park, shares that he has no remarks

121 [6:53:09 PM](#)

122 Open Public Hearing.



123 [6:53:23 PM](#)

124 Close Public Hearing.

125 [6:54:00 PM](#)

126 Commissioner Vaughan shares his thoughts that this is a straight forward project and therefore is in favor.

127 [6:54:28 PM](#)

128 COMMISSIONER **VAUGHAN** MOVED THE PLANNING COMMISSION RECOMMEND APPROVAL OF THIS  
129 AMENDMENT WITH THE CONDITION THAT THE CITY ENGINEER'S COMMENTS BE SATISFIED PRIOR TO THE PLATS  
130 RECORDATION. COMMISSIONER **MCCUISTION** SECONDS THE MOTION. THIS MOTION PASSES WITH  
131 UNANIMOUSLY.

132 [6:54:59 PM](#)

133 **7. Public Hearing – Proposed Amendment to §10.30.40 Animals**

134 Mr. Steele shares that a resident came to City Council and petitioned to change the Code to allow for more animals and  
135 be similar to that of Davis County's animal restrictions. Mr. Steele reads through the proposed amendments of the Code.

136 [6:58:17 PM](#)

137 Open Public Hearing.

138 [6:58:32 PM](#)

139 Close Public Hearing.

140 [6:58:44 PM](#)

141 Commissioner McCuiston shares concerns where the current code has 2 dogs and 4 cats and reducing that to 3  
142 animals total.

143 [6:59:54 PM](#)

144 Commissioner Vaughan feels that there should be some restrictions based on the zone and is not in favor of the proposed  
145 change. Commissioner Johnson shares that the change is good in reducing the number of animals and supports his statement  
146 with the distance requirements that are required currently with the kennel permits. Commissioner McCuiston shares his vote  
147 for approving the change. Commissioner Ruelan asks what the process is for the Code Enforcement officer when a complaint  
148 comes to the City. Mr. Davies gives a scenario for the Commission for clarification purposes.

149 [7:07:10 PM](#)

150 COMMISSIONER **JOHNSON** MOVED THE PLANNING COMMISSION RECOMMEND TO THE CITY COUNCIL  
151 APPROVAL FOR THE CHANGES TO THE ORDINANCE 10.30.40 ANIMALS AS WRITTEN. COMMISSIONER **RUELAN**  
152 SECONDS THE MOTION. THIS MOTION PASSES WITH A 5:1 VOTE.

153 [7:07:51 PM](#)

154 **8. Adjourn**

155 COMMISSIONER **MCCUISTION** MADE A MOTION TO ADJOURN TO WORK SESSION. COMMISSIONER  
156 **CRAGUN** SECONDED THE MOTION. ALL WERE IN FAVOR, THE MOTION CARRIED UNANIMOUSLY.

157  
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163 \_\_\_\_\_  
164 Brett Cragun, Chairman

Date Approved: \_\_\_\_\_

\_\_\_\_\_  
Commission Secretary

## Minutes of the Syracuse Planning Commission Work Session, November 6, 2018

Minutes of the Syracuse City Planning Commission Work Session held on November 6, 2018, at 6:00 p.m., in the Council Chambers, 1979 West 1900 South, Syracuse City, Davis County, Utah.

**Present:**

Commission Members:	Grant Thorson, Chair	
	Larry Johnson, Vice Chair	
	Brett Cragun	
	Curt McCuiston	
	Ralph Vaughan	
	Gretchen Ivy Ruelan	
City Employees:	Noah Steele, Community & Economic Development Director	
	Royce Davies, Planner	
	Heather Davies, Administrative Professional	
	Paul Roberts, City Attorney	
	Brian Bloeman, City Engineer	
	Jo Hamblin, Deputy Fire Chief	
City Council:	Councilwoman Corrine Bolduc	
	Councilman Dave Maughan	
	Councilman Jordan Savage	
Excused:	Dale Rackham	
	Greg Day	
Visitors:	Alan Prince	Jim & Marlene Ruggles
	Mike Wright	Kristie & Gordy Gerszewski
	Sharen Portillo	Nancy Ashby
	Cody Opperman	Ryan Thurgood
	Bennett Thurgood	Christian Jou
	Bonnie & Brent Bourgeois	

**7:10:07 PM**

Chair Thorson starts the Work Session.

**7:10:11 PM**

**1. Department Business:**

**a. City Council Liaison Report**

Councilwoman Bolduc shares:

- CC and PC was a combined meeting and therefore there is no new items to report.

**7:10:48 PM**

**b. City Attorney Updates**

Paul Roberts, City Attorney, stated that he has nothing.

**7:11:01 PM**

**c. Upcoming Agenda Items**

Mr. Davies shares the following items are as follows:

- Russon Mortuary Site Plan
- Still Water Phases 5 & 6
- Ninigret 3 Subdivision Amendment– Russon Subdivision
- No Meeting November 20<sup>th</sup>, December 18<sup>th</sup>, January 1<sup>st</sup>
- Election of Chair and approval of PC Calendar 2019 schedule

**7:12:56 PM**

**2. Discussion Items:**

None.

**7:13:03 PM**

**3. Commissioner Reports**

Mr. Vaughan shared that there wasn't much access for wheelchairs for City sponsored events. Mr. Vaughan shares the need to have everyone address people as Mr. and Mrs.

**7:15:55 PM**

**4. Adjourn**

COMMISSIONER **RACKHAM** MADE A MOTION TO ADJOURN. COMMISSIONER **JOHNSON** SECONDED. ALL WERE IN FAVOR. MOTION PASSES UNANIMOUSLY.

## Minutes of the Syracuse Planning Commission Regular Meeting, December 4, 2018

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Minutes of the Regular Meeting of the Syracuse City Planning Commission held on December 4, 2018, at 6:00 p.m., in the Council Chambers, 1979 West 1900 South, Syracuse City, Davis County, Utah.

**Present:** Commission Members: Grant Thorson, Chair  
Larry Johnson, Vice Chair  
Brett Cragun  
Curt McCuiston  
Greg Day  
Ralph Vaughan  
Gretchen Ivy Ruelan

City Employees: Noah Steele, Community & Economic Development Director  
Royce Davies, Planner  
Heather Davies, Administrative Professional  
Paul Roberts, City Attorney  
Brian Bloeman, City Engineer  
Jo Hamblin, Deputy Fire Chief

City Council: Councilwoman Corrine Bolduc

Excused: Dale Rackham

Visitors: **Matthew Russon**

[6:01:30 PM](#)

**1. Meeting Called to Order:**

Chair Thorson called the meeting to order. Commissioner Ruelan provided an invocation. The Pledge of Allegiance was led by Commissioner Vaughan.

[6:02:39 PM](#)

COMMISSIONER **VAUGHAN** MADE A MOTION TO ADOPT THE PLANNING COMMISSION AGENDA FOR THE DECEMBER 4, 2018 MEETING WITH THE CORRECTION OF THE ADDRESS ON THE AGENDA FOR STILL WATER PHASE 5 & 6 TO BE APPROX 2000 W 3000 S. THE MOTION WAS SECONDED BY COMMISSIONER **MCCUISTION**. ALL WERE IN FAVOR, THE MOTION CARRIED UNANIMOUSLY.

[6:03:21 PM](#)

**2. Meeting Minutes:**

COMMISSIONER **MCCUISTION** MADE A MOTION TO APPROVE THE MEETING MINUTES FOR OCTOBER 16, 2018 REGULAR MEETING AND WORK SESSION AS PUBLISHED. COMMISSIONER **VAUGHAN** SECONDED THE MOTION. ALL WERE IN FAVOR. THE MOTION CARRIED UNANIMOUSLY.

Chair Thorson noted that Commissioner Rackham is not present and Commissioner Ruelan will be voting for this meeting.

[6:04:20 PM](#)

**3. Public Comment:** This is an opportunity to address the Planning Commission regarding your concerns or ideas, regarding items that have not been scheduled for a public hearing on this agenda. Please limit your comments to three minutes.

Open public comment.

[6:04:38 PM](#)

Close public comment.

[6:04:41 PM](#)

**4. Public Hearing – Russon Brothers Mortuary Site Plan, located approx. 1550 W 300 S**

Mr. Royce Davies, shares that this is a commercial site plan located on a corner lot off SR-193 and 1550 W. This item was presented before the Architectural Review Committee which had a few items to be addressed. Mr. Davies gives a synopsis of the site plan. Staff recommends conditional approval for this item.

[6:11:02 PM](#)

Mr. Brent Russon, the applicant and owner, shares that they would love to expand to Syracuse and service this area.

[6:12:08 PM](#)

Open Public Hearing.

[6:12:16 PM](#)

Close Public Hearing.

64 [6:12:20 PM](#)

65 Commissioner Vaughan shares that both he and Commissioner Johnson both serve on the ARC. Commissioner  
66 Vaughan shares his appreciation for a cooperative applicant. Commissioner McCuistion is grateful for the Civil Engineer stamp  
67 on the drawings and site plan.

68 [6:14:11 PM](#)

69 COMMISSIONER **VAUGHAN** MOVED THE PLANNING COMMISSION APPROVE THE REQUEST OF RUSSON  
70 BROTHERS MORTUARY, GENERAL CORPORATION FOR A COMMERCIAL SITE PLAN FOR A MORTUARY LOCATED  
71 AT 1550 W 300 S IN THE BUSINESS PARK ZONE, ACCEPTING THE EXHIBITS AND FINDINGS IN THE STAFF REPORT  
72 WITH THE FOLLOWING CONDITION: 1. ALL REMAINING STAFF COMMENTS MUST BE ADDRESSED BEFORE A PRE-  
73 CONSTRUCTION MEETING CAN BE SCHEDULED INCLUDING PLANNING, ENGINEERING AND FIRE. COMMISSIONER  
74 **JOHNSON** SECONDED THE MOTION. THIS MOTION PASSES WITH A 5-1 VOTE.

75 [6:15:04 PM](#)

76 **5. Public Hearing – Ninigret North III Subdivision Amendment, located approx. 1550 W 300 S**

77 Mr. Davies shares that this is to divide a larger lot to allow for the Russon Mortuary and an additional lot to the west. Mr.  
78 Davies shares that his division of land will also include the road and a road stub to both the west and south.

79 [6:19:34 PM](#)

80 Open Public Hearing.

81 [6:19:44 PM](#)

82 Close Public Hearing.

83 [6:19:52 PM](#)

84 Commissioner Vaughan asks a clarifying question in regard to the Mortuary site plan.

85 [6:20:27 PM](#)

86 COMMISSIONER **DAY** MOVED THE PLANNING COMMISSION RECOMMEND THAT THE CITY COUNCIL  
87 APPROVE THE REQUEST OF NINIGRET CONSTRUCTION COMPANY NORTH, LCC FOR APPROVAL OF A SUBDIVISION  
88 PLAT CALLED NINIGRET NORTH III AT 1550 W 300 S IN THE BUSINESS PARK ZONE WITH THE FOLLOWING  
89 CONDITION: 1. ALL STAFF COMMENTS SHALL BE ADDRESSED BEFORE THE PLAT IS RECORDED WITH DAVIS  
90 COUNTY. COMMISSIONER **JOHNSON** SECONDED THE MOTION. THIS MOTION PASSES UNANIMOUSLY.

91 [6:21:30 PM](#)

92 **6. Final Plat – Still Water Phase 5 & 6, located approx. 2000 W 3000 S**

93 Mr. Davies shares the overview of where these are located in relation to the existing Still Water Phases. Mr. Davies  
94 shares that there is an added parcel to Phase 5 for a Davis County maintenance easement. Mr. Davies shares that there  
95 are some interesting addresses to some of the smaller lots on Phase 6. This will complete the northwest corner of the  
96 roundabout on 2000 W.

97 [6:25:56 PM](#)

98 Commissioner Vaughan asks some clarifying questions in regard to names and coordinates on the street addresses. MR.  
99 Davies shares that there are coordinates and indicates them on the plat.

100 [6:27:56 PM](#)

101 Commissioner Vaughan also questions when the construction of this project would be built as he is concerned that this  
102 could potentially be built at the same time as the school on the neighboring lot.

103 [6:28:56 PM](#)

104 Mr. Derek Terry, the applicant, shares that they intend to start the project hopefully either January or February of 2019.

105 [6:29:58 PM](#)

106 COMMISSIONER **VAUGHAN** MOVED THE PLANNING COMMISSION RECOMMEND TO THE CITY COUNCIL  
107 APPROVE THE REQUEST OF WOODSIDE HOMES FOR THE FINAL PLATS OF STILL WATER PHASES 5 AND 6  
108 LOCATED AT APPROX 2000 W 3000 S IN THE RPC ZONE WITH THE CONDITION THAT ALL STAFFS COMMENTS BE  
109 ADDRESSED PRIOR TO RECORDING WITH DAVIS COUNTY. COMMISSIONER **RUELAN** SECONDS THE MOTION. THIS  
110 MOTION PASSES WITH UNANIMOUSLY.

111 [6:30:52 PM](#)

112 **7. 2019 Meeting Schedule**

113 Commissioner Vaughan shares thoughts about keeping the September 3<sup>rd</sup> and December 17<sup>th</sup> meeting. Commissioner  
114 Johnson shares the same sentiments.

115 [6:33:10 PM](#)

116 COMMISSIONER **DAY** MOVED THE PLANNING COMMISSION ADOPT THE MEETING SCHEDULED AS  
117 PUBLISHED WITH THE CORRECTION TO INCLUDE SEPTEMBER 3<sup>RD</sup> AND DECEMBER 17<sup>TH</sup> MEETINGS.  
118 COMMISSIONER **VAUGHAN** SECONDS THE MOTION. THIS MOTION PASSES UNANIMOUSLY.

119 [6:33:35 PM](#)

120 **8. Election of Chairman and Vice-Chair**

121 There is discussion as to whom should be the next Planning Commission Chair.

122 [6:37:02 PM](#)

123 COMMISSIONER **JOHNSON** NOMINATES COMMISSIONER CRAGUN FOR CHAIR. THE MOTION WAS  
124 UNANIMOUS.

[6:38:48 PM](#)

COMMISSIONER **VAUGHAN** NOMINATES COMMISSIONER JOHNSON FOR VICE-CHAIR. THIS MOTION  
PASSES UNANIMOUSLY.

[6:39:45 PM](#)

**9. Adjourn**

COMMISSIONER **VAUGHAN** MADE A MOTION TO ADJOURN TO WORK SESSION. COMMISSIONER **JOHNSON**  
SECONDED THE MOTION. ALL WERE IN FAVOR, THE MOTION CARRIED UNANIMOUSLY.

\_\_\_\_\_  
Brett Cragun, Chairman

Date Approved: \_\_\_\_\_

\_\_\_\_\_  
Commission Secretary

DRAFT

## Minutes of the Syracuse Planning Commission Work Session, December 4, 2018

Minutes of the Syracuse City Planning Commission Work Session held on December 4, 2018, at 6:00 p.m., in the Council Chambers, 1979 West 1900 South, Syracuse City, Davis County, Utah.

**Present:**

Commission Members:	Grant Thorson, Chair Larry Johnson, Vice Chair Brett Cragun Curt McCuiston Greg Day Ralph Vaughan G. Ivy Ruelan
City Employees:	Noah Steele, Community & Economic Development Director Royce Davies, Planner Heather Davies, Administrative Professional Paul Roberts, City Attorney Brian Bloeman, City Engineer Jo Hamblin, Deputy Fire Chief
City Council:	Councilwoman Corrine Bolduc
Excused:	Dale Rackham
Visitors:	

6:40:00 PM

Chair Thorson starts the Work Session.

7:40:08 PM

**1. Department Business:**

**a. City Council Liaison Report**

Councilwoman Bolduc shares:

- Shares that they discussed the IBI Study
- The Council approved the annexation for the CW Land/Church Property
- The Council tabled the annexation item that is on the Work Session for tonight's meeting

6:42:19 PM

**b. City Attorney Updates**

Paul Roberts, City Attorney, stated that he has nothing.

6:42:24 PM

**c. Upcoming Agenda Items**

Mr. Davies shares the following items are as follows:

- No meetings December 18<sup>th</sup> or January 1<sup>st</sup>. Next meeting January 15<sup>th</sup>.
- IBI Presentation
- Shoreline Subdivision – TBD
- Thought/Pledge Schedule
- Distant Serenade Subdivision – TBD

6:44:09 PM

**2. Discussion Items:**

**a. §10.35.040 Home Occupation**

Mr. Steele shares that this Code change came from a resident and business owner in Syracuse. They have petitioned the City Council to amend the Home Occupation code to allow for more vehicles and special exemptions to the Standards for the location of his home and lot size. City Council would like them to review and give a recommendation in regard to this Code change.

6:51:10 PM

Commissioner Vaughan shares that there is a large number of vehicles on this specific property and does agree that there are too many vehicles. Commissioner Vaughan shares his thoughts that Home Occupations are incubators to help businesses start up and move to Commercial. Commissioner Vaughan shares his opinion in not wanting to change the code to accommodate one business.

6:55:23 PM

Commissioner Day shares that in his neighborhood there are a lot of camping trailers that are on home owner's driveways that are not regulated. Commissioner Johnson shares his thoughts in requiring that applicant to park the

vehicles in the backyard and provide adequate screening for larger lots about 1 acre or more. There is discussion between travel trailers and commercial trucks/trailers.

7:00:20 PM

Commissioner McCuiston brings up the point of having large commercial vehicles in residential areas that can cause nuisance including noise, waste and sight impairments. Commissioner Ruelan asks what the City's ways of handling a violation of the Code. Mr. Paul Roberts, City Attorney, gives the rundown process of how to go about the violation. Commissioner Day asks for clarification as to how other cities deal with this sort of issue.

7:05:35 PM

Commissioner McCuiston questions the number of trucks/drivers for vehicles. Commissioner Johnson and Commissioner Day shares the same opinion as Commissioner McCuiston. Chair Thorson shares his thoughts in seeing there isn't much wiggle room to change the Code. Commissioner Vaughan shares his thoughts that he would change the code if the home was located on a 4-lane road and placed behind a 6 ft fence and behind the front setback of the home and otherwise be a Major Use.

7:12:28 PM

**b. Proposed David George Annexation, located approx. 3800 W 3000 S**

Mr. Steele gives a synopsis of how this proposed annexation has come to pass over the last 4 years. Mr. Steele shares pros and cons for annexing this land into the City.

7:19:20 PM

Commissioner McCuiston questions where the current zoning of the City is. Commissioner Johnson shares concerns for having utilities, specifically sewer and water, for this location.

7:21:30 PM

**3. Commissioner Reports**

Chair Thorson has some questions concerning a gated community proposed for the Stillwater Lake Estates.

Commissioner Ruelan shares that she was contacted to have trees in front of the schools along 2000 W. Commissioner Vaughan wishes everyone Happy Holidays.

7:26:59 PM

**4. Adjourn**

COMMISSIONER JOHNSON MADE A MOTION TO ADJOURN. COMMISSIONER MCCUISTION SECONDED THE MOTION. ALL WERE IN FAVOR. MOTION PASSES UNANIMOUSLY.





# PLANNING COMMISSION AGENDA

January 15, 2019

## **Agenda Item # 3**

### **Public Comment:**

This is an opportunity to address the Planning Commission regarding your concerns or ideas, regarding items that have not been scheduled for a public hearing on this agenda. Please limit your comments to three minutes.



# PLANNING COMMISSION REGULAR MEETING

January 15, 2019

Agenda Item # 4

## **Consideration of Annexation Petition, located approximately 3800 W 3000 S (Legislative Decision)**

### *Summary*

-Original applicaiton: David George, 57 acres, July 2, 2014

PC Action: 07/15/14, 08/5/14 - reccommend denial because of utility, wetland, access, concerns

CC Action: 8/12/14 - tabled until utility concerns resolved

-Amended application acreage & date: David George & Stevan Vasic, 117 acres, April 14, 2017

PC Action: none

CC Action: April 25, 2017 - tabled until UDOT issues resolved

-After negotiations with UDOT, it is currently proposed to annex 60.779 acres

The developers would like to resume annexation of the remaining property after working with UDOT. It is proposed to develop using the R-1 Cluster ordinance which is not consistent with the general plan map. The GP map designates it as A-1. Also, some of the initial concerns remain among staff concerning the suitability of development on the property. It is recommended that the 9 criteria for annexation found in our adopted annexation policy plan be considered. (see following page) . This property is within the city's future annexation plan. Please especially consider criteria numbers 3, 5, and 7 below:

3. Systematic Growth: the closest subdivision to the east is one mile away, there is no development south or west. Tuscany meadows in north east of the property.
5. Population Growth: Our population is estimated to double by 2050, housing is needed, as are open spaces and trails. The trails and open spaces proposed in conjunction with the concept plan could assist in maintaining the parks LOS.
7. Financial requisites for municipal services: 2700 S is a two lane country road without sidewalk or curb and gutter. If the city is required to extend curb gutter, sidewalk, and widening of the road beyond this project's boundaries, that would be costly. It is unknown if the county will require the road to be dedicated to the city. The development will also bring in property tax and impact fees.

Seeing that this application has been active since 2014, it is recommended that timely action be taken on this item.

# **Syracuse City Annexation Policy Plan**

**November 26, 2002**

**STATEMENT OF PURPOSE.** The City of Syracuse has determined it to be necessary to manage and access the impact and implementation of annexation goals of the Syracuse City Annexation Policy Plan and to promote health, safety, and welfare of the City.

**CRITERIA FOR ANNEXATION.** Syracuse City will favor a petition for annexation of territory in the areas outlined in “Appendix A” and pursuant to the following criteria. The City, in acceptance of an annexation petition shall confirm:

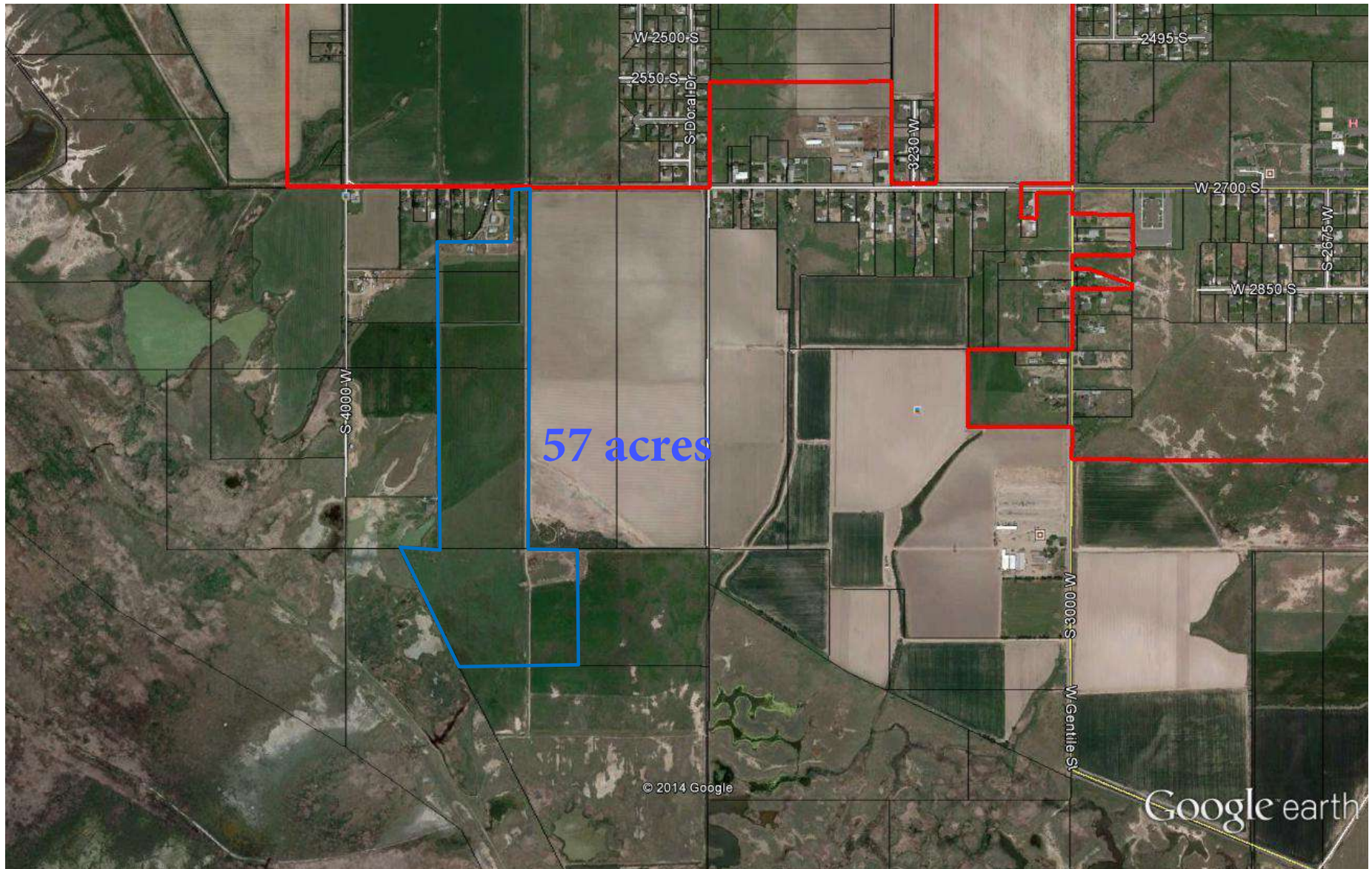
1. The petition for annexation complies with Utah State Code 10-2-403.
2. Promotes the goals of government to protect and promote the public health, safety and general welfare of the citizens of Syracuse, present and future.
3. Encourages systematic growth and development within the City and the keeping of a cohesive and orderly community.
4. Considers in conjunction with the Syracuse City General Plan the need over the next 20 years for additional land suitable for residential, commercial and industrial development.
5. Considers population growth projections for Syracuse City and adjoining areas for the next 20 years.
6. Assures availability, maintenance, extension and/or adequate capacity of public facilities and services.
7. Considers the City’s future and current financial requisites for municipal services in developed and undeveloped unincorporated areas of Davis County.
8. Promotes the most efficient relationships between land uses in Syracuse City and its neighboring communities and service districts by avoiding gaps or overlaps with expansion areas of other municipalities.
9. Ensures the pace and quality of annexations shall be within the management capability of Syracuse City by the use of well-conceived land management practice.



# Annexation Request (2014)

## 3807 W 2700 S

### Siefert Meadows, LLC



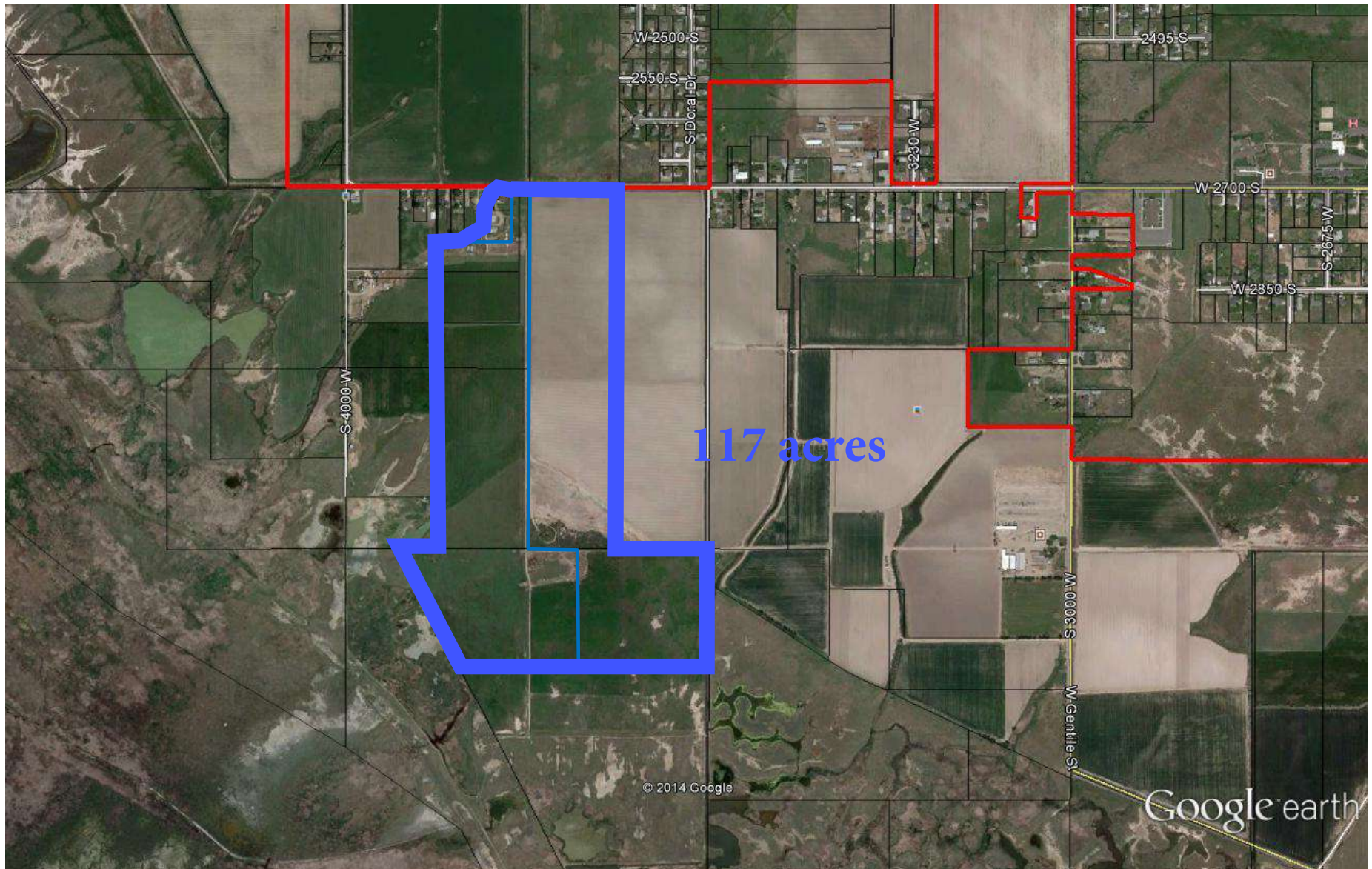




# Annexation Request (2017)

## 3807 W 2700 S

### Siefert Meadows, LLC



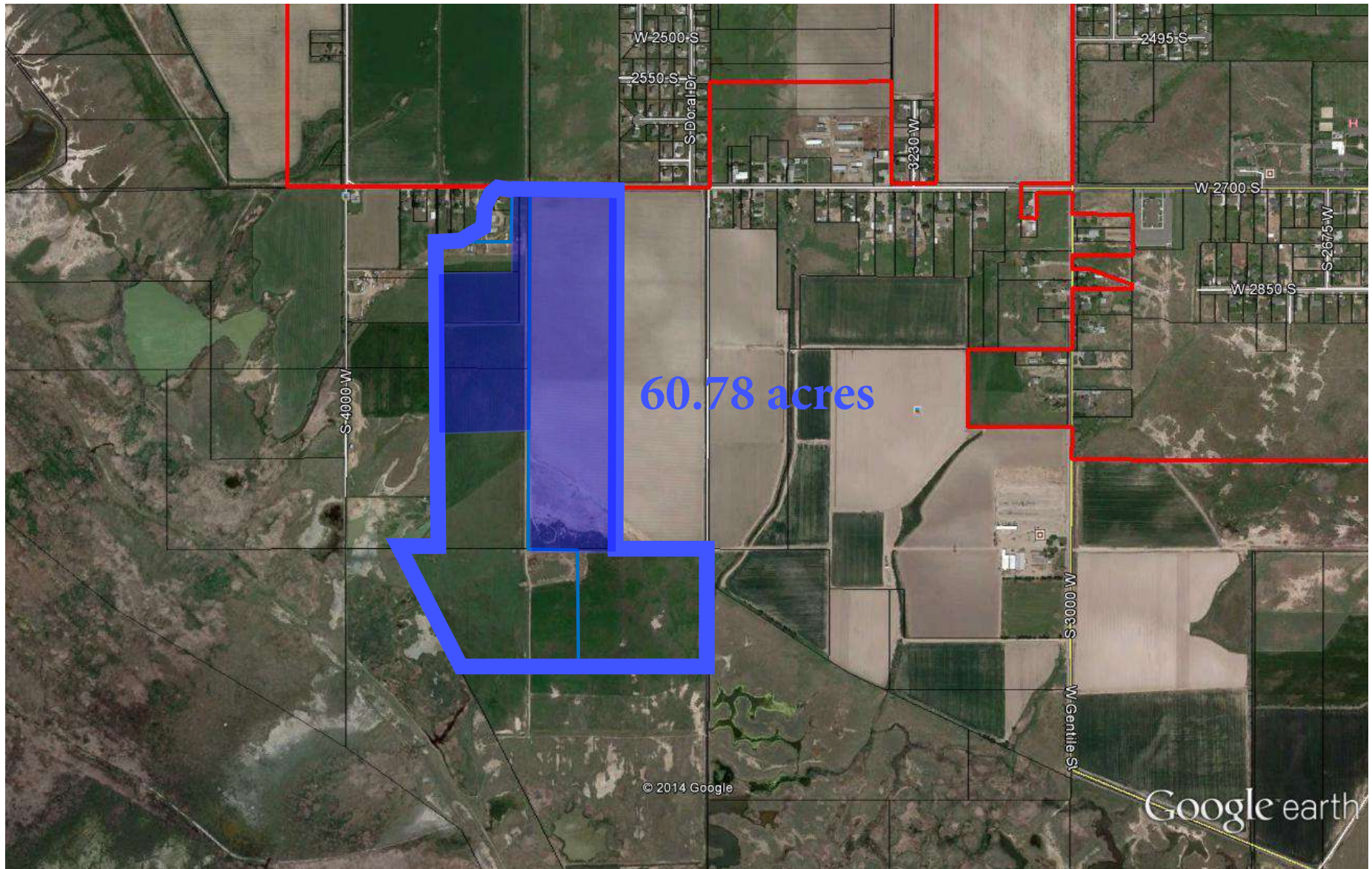




# Annexation Request (2019)

## 3807 W 2700 S

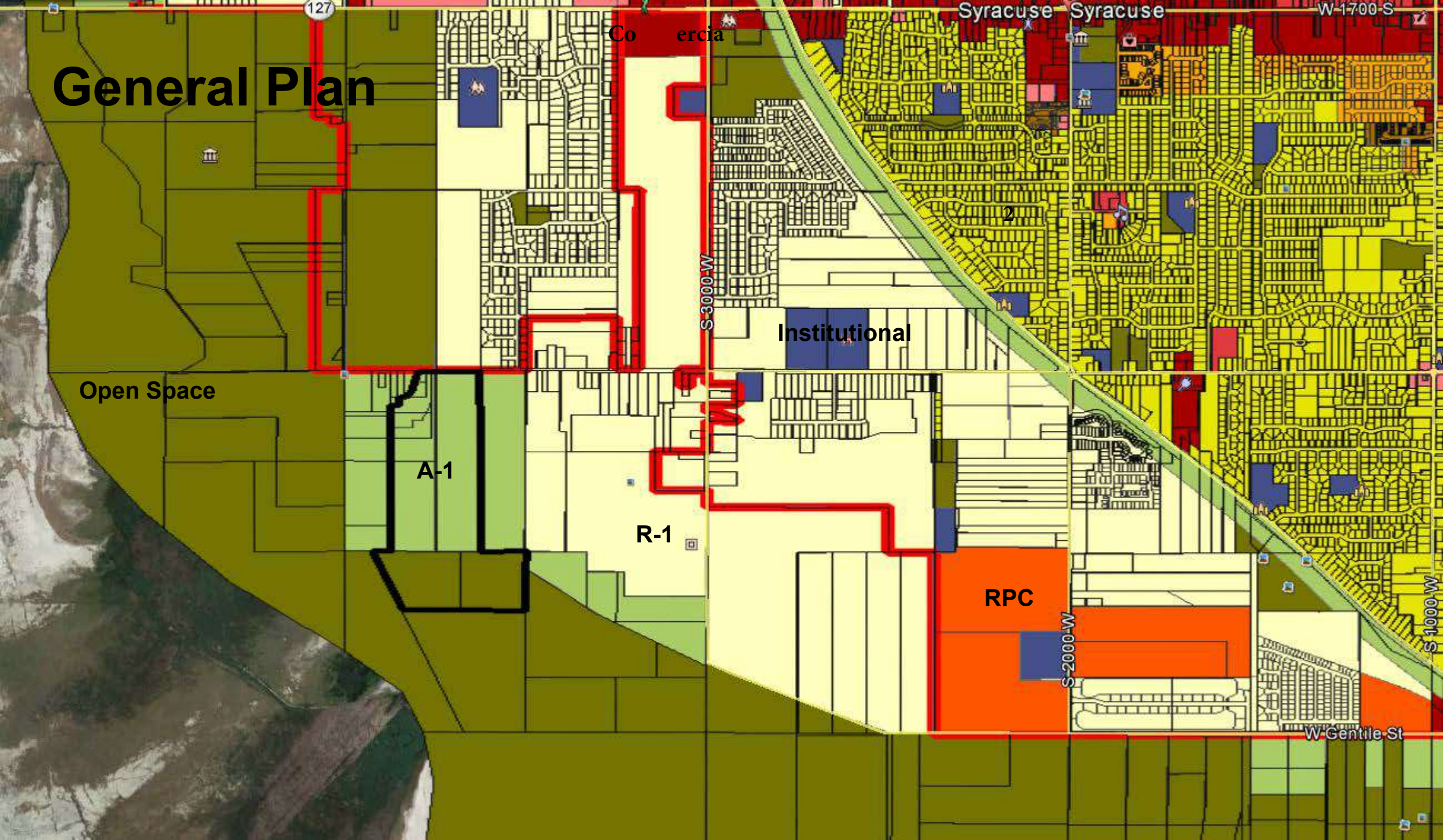
### Siefert Meadows, LLC







# General Plan





#### 10.50.020 Zoning of territory annexed into the City.

The City Council, with recommendations by the Planning Commission, shall determine at the time of annexation the most appropriate zoning of all property hereafter annexed into Syracuse City. Notwithstanding SCC 10.20.070, and unless otherwise provided in the annexation approval, the property shall be designated as A-1 at time of annexation.

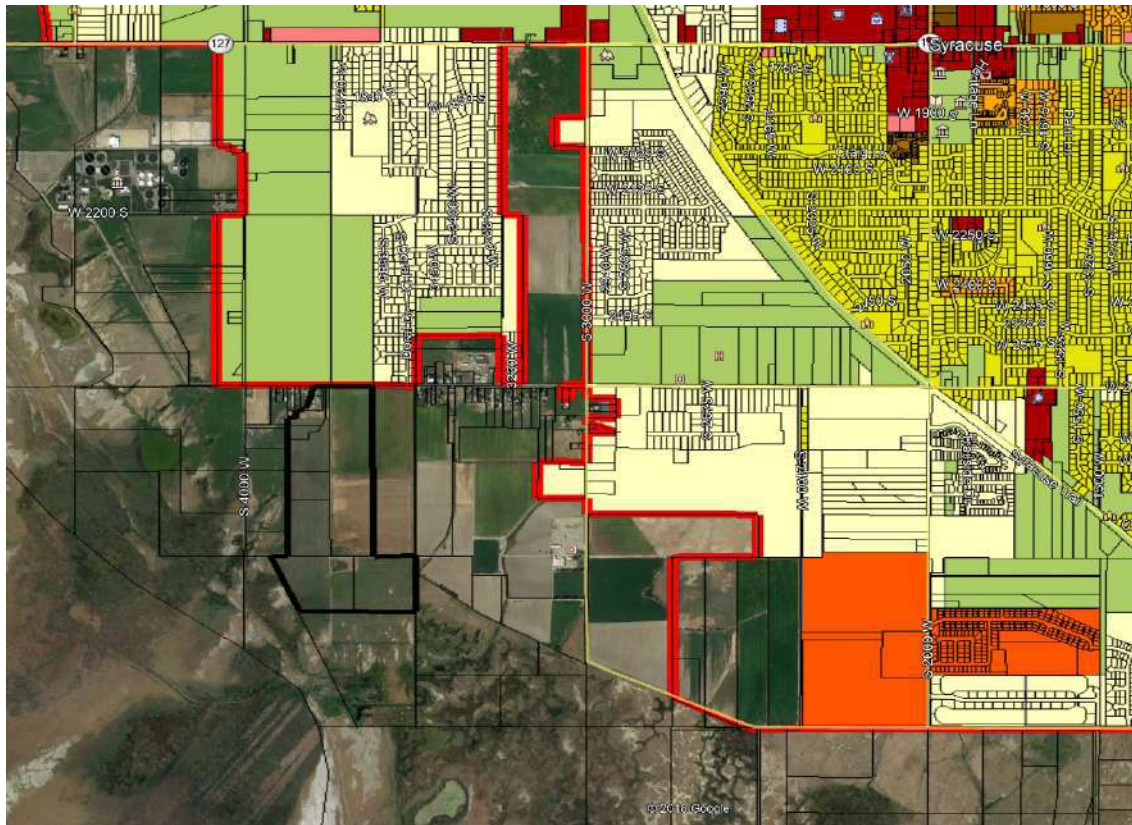
Annexation application	Planning Commission (with recommendation of zoning designation)	City Council (public hearing and notice required)	(If petition or ordinance is denied, process ends)	(If petition or Ordinance is denied, process ends)
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#### 10.20.070 Zoning map and text amendments.

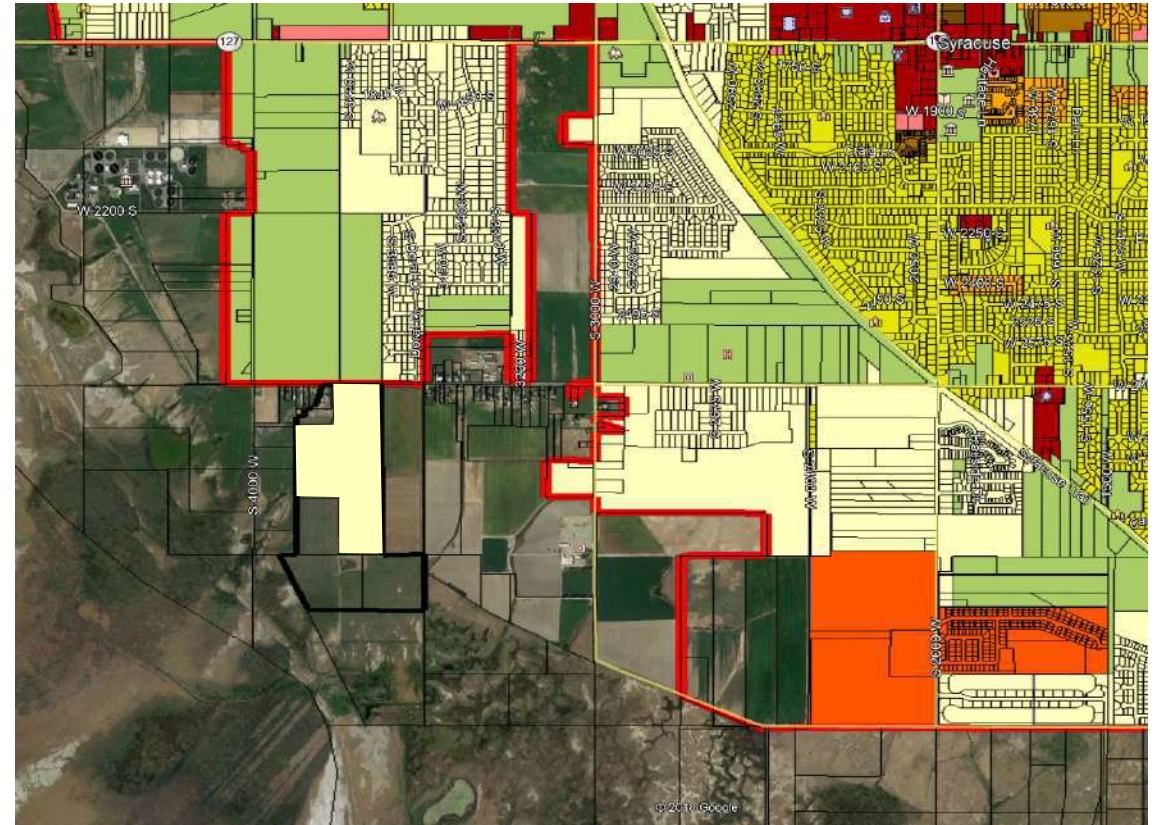
(E) Approval Standards. A decision to amend the text of this title or the zoning map is a matter of legislative discretion by the City Council and not controlled by any one standard. However, such changes shall be consistent with the current general plan and general plan map, and in making an amendment, the City Council should also consider:

- (1) Whether it would be harmonious with the overall character of existing development in the vicinity of the subject property, or in cases of text amendments, in areas governed by the amended text;
- (2) Whether it would be consistent with the standards of any applicable overlay zone and, in cases of text amendments, harmonious with areas governed by the amended text;
- (3) The extent to which it may adversely affect adjacent property; and
- (4) The adequacy of facilities and services intended to serve the subject property, including but not limited to roadways, parks and recreation facilities, police and fire protection, schools, storm water drainage systems, water supplies, and waste water and refuse collection.

Existing Zoning



Proposed Zoning

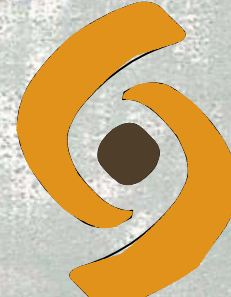






**civilsolutionsgroup** inc.

CACHE VALLEY | P: 435.213.3762  
SALT LAKE | P: 801.216.3192  
UTAH VALLEY | P: 801.874.1432  
info@civilsolutionsgroup.net  
www.civilsolutionsgroup.net





## Annexation questions

- How much of the property can be serviced by gravity sewer? We don't allow sewer pumps.
  - All of the lots will be gravity flow
  - How many feet of fill need to be brought in to make the sewer drain north?
  - We should have all the materials we need onsite
  - What are the chances that the fill will settle leaving the roads and foundations in disrepair?
  - Foundations will generally be on undisturbed Native soils
  - Roads will be on required Road Base, subgrade etc.... per the City standards and Soils report
  - According to the Geo Tech report this should not be a problem as long as compaction is done according to the report
  - Will the fill create wetlands and runoff issues for the neighboring properties?
  - No, it will not, however most of the neighboring property will be wetlands (UDOT)
  - Will the county require that the city take over ownership/maintenance of 2700 S in order to service this development?
  - We don't think so, but I have not gotten a definite answer from the county yet
  - Will 2700 S need to be widened/improved to service this development?
  - No, however we will widen 2700 S Along our Frontage
  - Will the following public items/utilities need to be extended to service the residents of the proposed project? If yes, how far is the nearest location? Sewer, Secondary Water, Culinary Water, Power, Gas, Sidewalk, Storm Drain, Curb and Gutter, Internet These services are all available in 2700 along our frontage and are sufficient to service the property
  - What added costs to taxpayers will annexing this property result in?
  - There will be increased recurring revenue as well as impact and building fees. Attached are estimates of those fees and recurring revenue
  - What benefit to the city does annexing this property provide? This development will provide much needed single-family homes with varying prices and with great amenities for the residents. Trails, hiking, biking, paddle boarding, kayaking, fishing and many more....
-

Building fees	
Plan Check Fee	\$ 478.00
Building Value fee	\$ 1,196.00

Impact fees	
Parks Trails and Recreation	\$ 2,393.56
Transportation	\$ 743.00
Culinary water 3/4 meter	\$ 805.00
Secondary water	\$ 3,085.00

Connection fees	
3/4 inch meter	\$ 485.00
Sewer	\$ 300.00

Total Fees Per homesite	\$ 9,485.56
-------------------------	-------------

Total of fees for the development

Recurring Revenue Property tax per homesite to Syracuse	
Tax rate Syracuse City	0.001512
Average valuation	\$ 500,000.00
Taxable value	\$ 275,000.00
Annual property tax To Syracuse	\$ 756.00

Total annual property tax per homesite	\$ 3,537.88
--	-------------

**General Plan Amendment**  
for  
Private Lake Estates llc  
3592 South Redwood Road  
West Valley City UT 84119  
801 860 7793

**Written Statement for Potential/Proposed Amendment**

We propose to develop the property into residential units with an R-1 zoning. We are proposing a cluster of homes to fit within the density rules for the R-1 zoning, a private water ski lake (similar to the Still Water development on 2000 west Gentile street in Syracuse), and other home sites around the lake with approximately 1/3 acre lots. Additionally, we plan to develop some other open space amenities, a side-by-side Razor motorcycle track, and a 9-hole golf course and/or soccer/lacrosse fields. The amenities will be managed and maintained by the homeowners association.

**Written Statement Why the General Plan is no longer feasible**

The properties are currently being used as farmland. We believe that this is no longer the best use of this land. With the shortage of housing along the Wasatch Front, we feel these properties would better serve the community as residential lots with amazing amenities.

**Potential Impacts**

**Traffic** Single Family homes generate about 10 trips per day we plan to develop approximately 200 units = 2,000 trips per day

**Streets** The subdivision will be accessed through 2700 south, the county road to the north of the project. Approximately 3 miles of roads will be donated to the city.

**Intersections** The intersection for the subdivision will be on 2700 south.

**Water and Sewer** The water and sewer will extend from 2700 south. The sewer shall have a minimum slope of 0.50% with an 8" pipe. We will build up the lots to the south to allow for proper drainage.

Primary water use will be approximately 200 gpd x 200 homes = 40,000 gpd.  
Secondary water use will be approximately 680 gpd for the 1/3 acre lots x 34



homes = 23,120 and an additional 75,000 gpd for open spaces for the cluster homes.

Sewer is a function of indoor water use. We would assume that around 90-95% of the indoor use is drained into the sewer = 38,000 gpd.

**Storm Drains** Storm runoff varies between 1.5 cfs/acre to 2.25 cfs/acre depending on the storm year we will capture the runoff in the lake and in the other open spaces.

**Electrical Power** Electricity will be established from the existing power line on 2700 south.

**Fire Protection** Nearby fire department is located at 3000 west.

**Garbage Collection** Garbage collection will be expanded to the new homes.

**Other City Matters** The Amenities planned for the development will help to beautify the city and will be managed by the homeowners association.



**Mayor**  
Terry Palmer

**City Council**  
Jordan Savage  
Mike Gailey  
Corinne Bolduc  
Andrea Anderson  
David Maughan

**City Manager**  
Brody Bovero

July 10, 2017

Executive Director  
Utah Department of Transportation  
4501 South 2700 West  
P.O. Box 141245  
Salt Lake City, UT 84114-1245

Mr. Braceras,

This is written notice that we have received an application for an annexation of approximately 117 acres on property located at approximately Doral Drive and 2700 South, Syracuse, Utah 84075, parcel numbers 12-102-0058, 12-102-0057, 12-102-0096, 12-102-0095, 12-102-0091, 12-102-0088, and 12-102-0087. The proposed annexation is located within the boundaries of a high priority transportation corridor for the West Davis Corridor.

In accordance with Utah State Code Title 10 Chapter 9a Part 5 Section 509 we anticipate that receipt of this notice initiates the 45 day waiting period, allowing Syracuse City to move forward with entitlements providing UDOT does not contact us to initiate alternative procedures.

If you have any questions concerning this development, please contact me by phone at 801-614-9632 or by email at [rdavies@syracuseut.com](mailto:rdavies@syracuseut.com).

Regards,

Royce Davies  
Syracuse City Planner



State of Utah

GARY R. HERBERT  
*Governor*

SPENCER J. COX  
*Lieutenant Governor*

## DEPARTMENT OF TRANSPORTATION

CARLOS M. BRACERAS, P.E.  
*Executive Director*

SHANE M. MARSHALL, P.E.  
*Deputy Director*

July 19, 2017

Mr. Royce Davies  
Syracuse City Planner  
Syracuse City  
1979 West 1900 South  
Syracuse, UT 84075

Dear Mr. Davies:

A representative from our staff will be contacting you to learn more details regarding the annexation request regarding the 235 acres of property located at approximately Doral Drive and 2700 South in Syracuse.

Sincerely,

A handwritten signature in blue ink, appearing to read "Kris T. Peterson".

Kris T. Peterson, P.E.  
Region One Director

KTP:jkm

Cc: Carlos Braceras, Executive Director  
Randy Jefferies, Project Director

**From:** Randy Jefferies <[rjefferies@utah.gov](mailto:rjefferies@utah.gov)>

**Sent:** Wednesday, September 19, 2018 5:47 PM

**To:** Brody Bovero <[bbovero@syracuseut.com](mailto:bbovero@syracuseut.com)>

**Cc:** Stevan Email <[stevanthegreat@gmail.com](mailto:stevanthegreat@gmail.com)>; David George <[d.george60@gmail.com](mailto:d.george60@gmail.com)>; Harris, Rex <[rexharris@utah.gov](mailto:rexharris@utah.gov)>

**Subject:** 2700 S Ski Lake Annexation

Brody,

Stevan Vasik, David George, and I have been coordinating on their proposed development. As you know, they had initially proposed a 117 acre annexation with ski lake, golf course, and ATV track

(shaded yellow area below). Much of that property, about 59 acres, conflicted with the property UDOT needs for WDC wetlands mitigation (light blue outline). Through discussions with the developers and the Corps of Engineers, we have flexibility to make a 150' wide strip available for the development as shown in dark blue below. According to Stevan, this would provide enough room for the ski lake portion of the development. I am sending this email to you at Stevan's request so that the city knows it can consider his ski lake proposal independent of the WDC mitigation requirements. Please contact me if you have any questions.

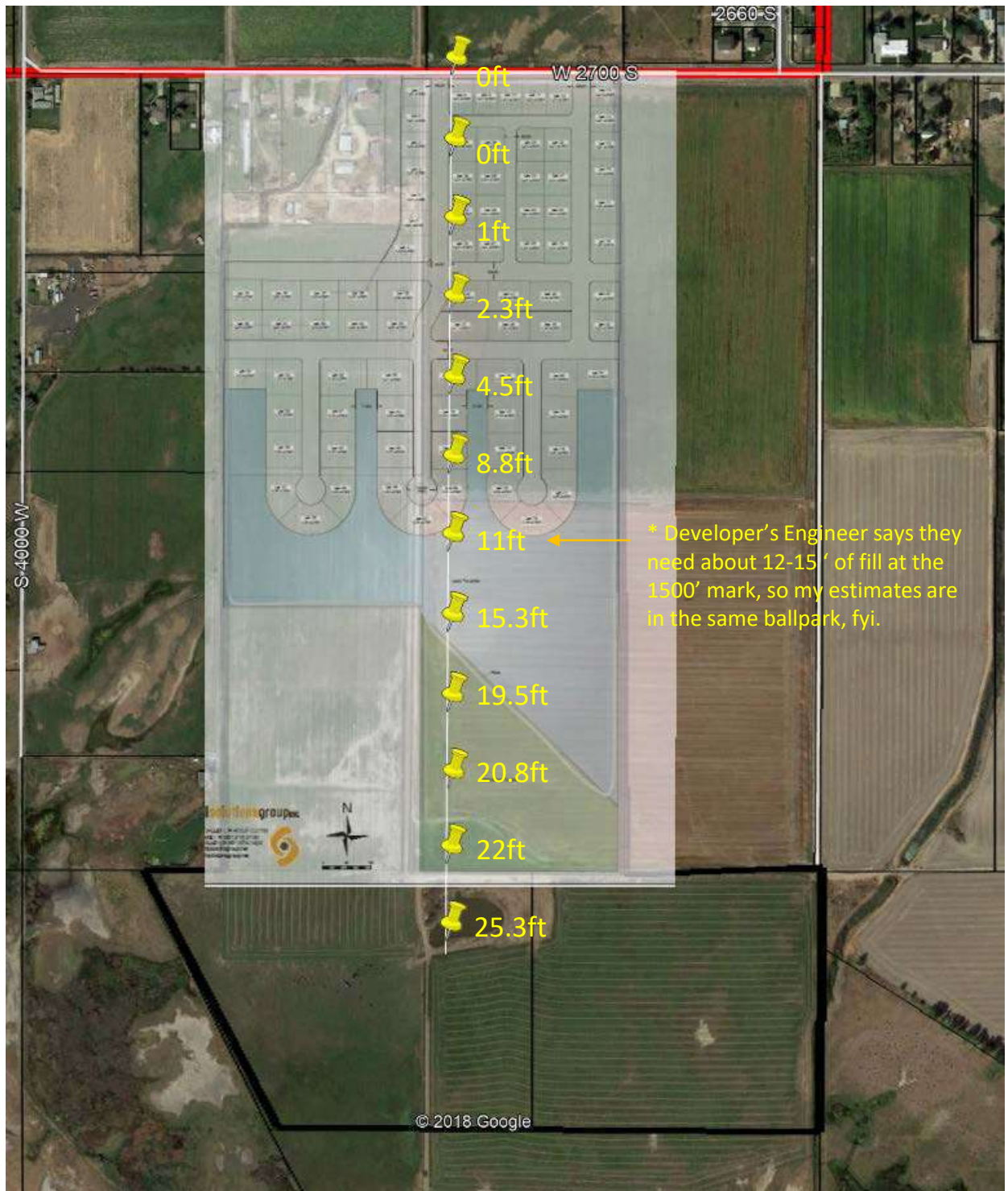
Randy Jefferies  
UDOT Region 1



## Approximate Needed Fill Needed for Gravity Sewer

Assuming a .5% rise, using existing elevations, and assumed 6' pipe cover , starting elevations derived from google earth and are not survey grade.

\*This is for demonstration purposes only. More detailed engineering and geotechnical work would be provided by the developer should this property be annexed.







# Pond

Road Surface  
New Sewer Pipe

## Existing Grade



point	feet from start (run in feet)	Surface Elevation	Pipe Elevation	Ft pipe out of the ground	Feet finished grade above natural grade (assumed 6ft pipe cover)	Pipe Depth below surface (ft)
1	0	4225	4217.5	-7.5	0	7.5
2	250	4225	4219	-6.3	-0.3	6
3	500	4225	4220	-5.0	1.0	6
4	750	4225	4221	-3.8	2.3	6
5	1000	4224	4223	-1.5	4.5	6
6	1250	4221	4224	2.8	8.8	6
7	1500	4220	4225	5.0	11.0	6
8	1750	4217	4226	9.3	15.3	6
9	2000	4214	4228	13.5	19.5	6
10	2250	4214	4229	14.8	20.8	6
11	2500	4214	4230	16.0	22.0	6
12	2750	4212	4231	19.3	25.3	6





# PLANNING COMMISSION REGULAR MEETING

January 15, 2019

## ADMINISTRATIVE ITEM

Agenda Item # 5

Criddle Farms North Phase 2 Final  
4050 West 920 South

### *Factual Summation*

Please review the following information. Any questions regarding this agenda item may be directed to Royce Davies, City Planner or Noah Steele, CED Director.

Location:	4050 West 920 South
Current Zoning:	R-3
General Plan:	R-3
Total Subdivision Area:	10.552 Acres
Number of Lots:	33

### *Summary*

The applicant has requested approval of a thirty-three-lot subdivision as an addition to the existing Criddle Farms development. This phase will complete the Criddle Historical Park property creation and be the first part of Criddle Farms to utilize utilities outlined in an interlocal agreement between Syracuse and West Point.

Staff reviews for a previous version of the plat have been included in this report, but the plat provided addresses all comments according to the applicant. Staff has not had time to review the plat to verify that comments have been adequately addressed. The comments provided do not substantially change any parts of the plat if addressed. A review from the city of West Point has been included in accordance with the Criddle Farms development agreement.

### *Recommendation*

Because there are outstanding staff comments on the plat and the comments do not substantially change any parts of the plat, staff recommends it be **conditionally approved**.

### ***Suggested Motion Language***

Approval – “I move the Planning Commission recommend the City Council approve the request of Criddle Farms North Phase 2 for approval of a subdivision plat called Criddle Farms North Phase 2 at 4050 West 920 South in the R-3 Zone with the following condition:

1. All staff comments shall be addressed before the plat is recorded with Davis County.

Table – “I move the Planning Commission continue the request of Criddle Farms North Phase 2 for approval of a subdivision plat called Criddle Farms North Phase 2 at 4050 West 920 South in the R-3 Zone until (give date) based on the following findings:

1. (list findings)”

Denial – “I move the Planning Commission deny the request of Criddle Farms North Phase 2 for approval of a subdivision plat called Criddle Farms North Phase 2 at 4050 West 920 South in the R-3 Zone based on the following findings:

1. (list findings).”

### ***Attachments:***

- Aerial Map
- General Plan Map
- Zoning Map
- Plat
- R-3 Zoning Ordinance
- Staff Reviews



Criddle Farms North Phase 2

4050 West 920 South

Legend

 Criddle Farms North 2 Outline





# Criddle Farms North Phase 2 General Plan

4050 West 920 South

Legend

Criddle Farms North 2 Outline

Institutional

R-1 (2.90 dwellings per net acre)

R-2 (3.79 dwellings per net acre)

R-3 (5.44 dwellings per net acre)



# Criddle Farms North Phase 2 Zoning

4050 West 920 South

Legend

Criddle Farms North 2 Outline

R-1 (2.90 dwellings per net acre)

R-2 (3.79 dwellings per net acre)

R-3 (5.44 dwellings per net acre)



600 ft





# CRIDDLE FARMS NORTH SUBDIVISION PHASE 2

900 SOUTH TO 1150 SOUTH 4000 WEST STREET  
SYRACUSE, UTAH

FOR REVIEW  
NOT FOR CONSTRUCTION

DATE PRINTED  
January 11, 2019

INDEX OF DRAWINGS

1 OF 2	SUBDIVISION PLAT
2 OF 2	SUBDIVISION PLAT
C-001	GENERAL NOTES
C-100	SITE PLAN
C-101	HISTORIC PARK SITE/LANDSCAPE PLAN
C-200	GRADING AND DRAINAGE PLAN
C-300	UTILITY PLAN
C-400	EROSION CONTROL PLAN
C-500	DETAILS

PP-1	4050 WEST STREET PLAN AND PROFILE
PP-2	4050 WEST STREET PLAN AND PROFILE
PP-3	920 SOUTH STREET OFFSITE PLAN AND PROFILE
PP-4	920 SOUTH STREET OFFSITE PLAN AND PROFILE
PP-5	920 SOUTH STREET PLAN AND PROFILE
PP-6	920 SOUTH STREET PLAN AND PROFILE
PP-7	4110 WEST STREET AND 870 SOUTH STREET PLAN AND PROFILE
PP-8	4230 WEST STREET PLAN AND PROFILE
PP-9	4500 WEST STREET PLAN AND PROFILE

NOTICE TO CONTRACTOR

ALL CONTRACTORS AND SUBCONTRACTORS PERFORMING WORK SHOWN ON OR RELATED TO THESE PLANS SHALL CONDUCT THEIR OPERATIONS SO THAT ALL EMPLOYEES ARE PROVIDED A SAFE PLACE TO WORK AND THE PUBLIC IS PROTECTED. ALL CONTRACTORS AND SUBCONTRACTORS SHALL COMPLY WITH THE OCCUPATIONAL SAFETY AND HEALTH REGULATIONS OF THE U.S. DEPARTMENT OF LABOR AND THE STATE OF UTAH DEPARTMENT OF INDUSTRIAL RELATIONS CONSTRUCTION SAFETY ORDERS. THE CIVIL ENGINEER SHALL NOT BE RESPONSIBLE IN ANY WAY FOR THE CONTRACTORS AND SUBCONTRACTORS COMPLIANCE WITH SAID REGULATIONS AND ORDERS.

CONTRACTOR FURTHER AGREES TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB-SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY. THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND THE CIVIL ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR ENGINEER.

DEVELOPER:

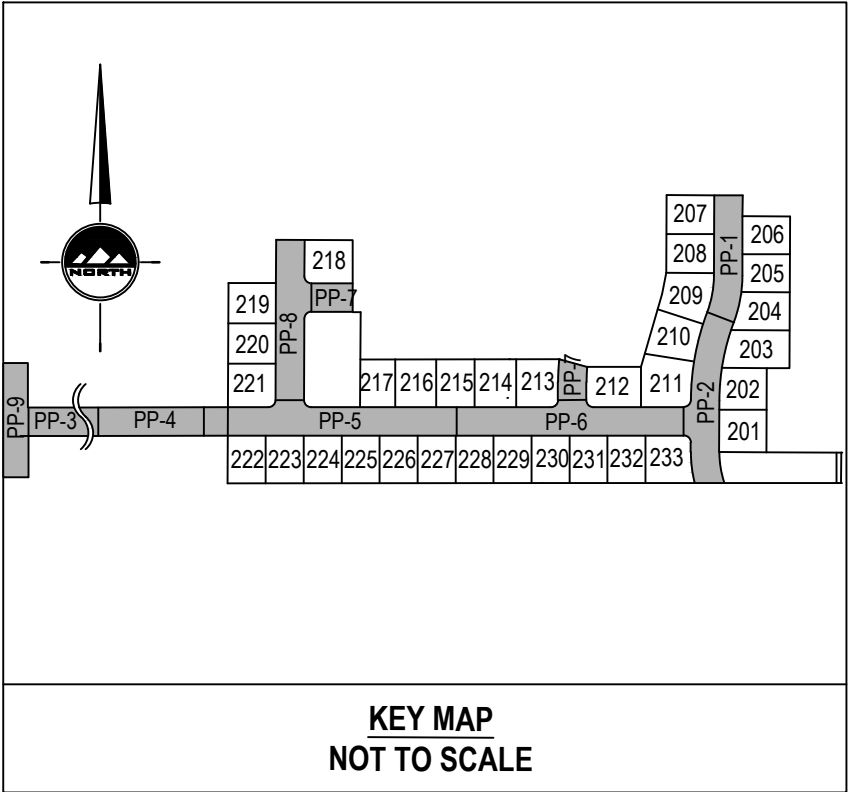
DR HORTON, INC.  
12351 S GATEWAY PARK PLACE  
DRAPER, UTAH 84020  
SUITE D-100  
ADAM LOSER  
801-571-7101  
arloser@drhorton.com

NOTICE TO DEVELOPER/ CONTRACTOR

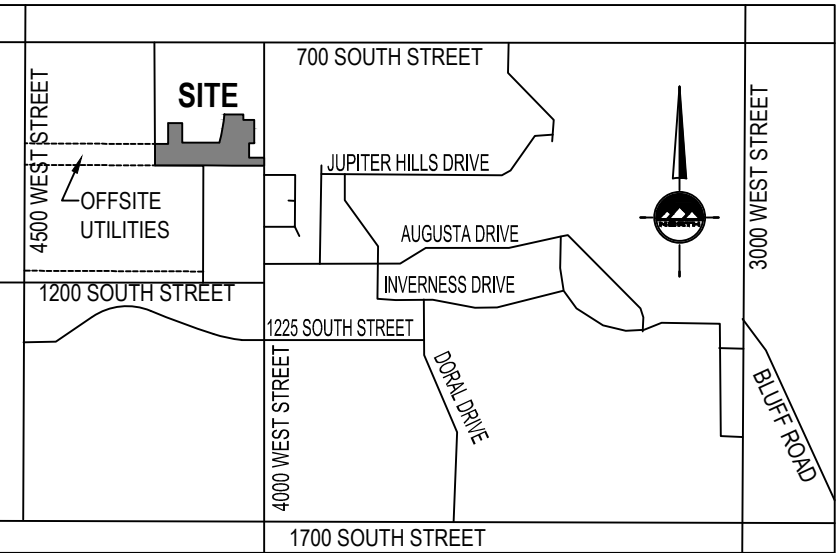
UNAPPROVED DRAWINGS REPRESENT WORK IN PROGRESS, ARE SUBJECT TO CHANGE, AND DO NOT CONSTITUTE A FINISHED ENGINEERING PRODUCT. ANY WORK UNDERTAKEN BY DEVELOPER OR CONTRACTOR BEFORE PLANS ARE APPROVED IS UNDERTAKEN AT THE SOLE RISK OF THE DEVELOPER, INCLUDING BUT NOT LIMITED TO BIDS, ESTIMATION, FINANCING, BONDING, SITE CLEARING, GRADING, INFRASTRUCTURE CONSTRUCTION, ETC.

UTILITY DISCLAIMER

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND / OR ELEVATIONS OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.



VICINITY MAP



GENERAL NOTES

- ALL WORK SHALL CONFORM TO SYRACUSE CITY STANDARDS & SPECIFICATIONS.
- CALL BLUE STAKES AT LEAST 48 HOURS PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION ACTIVITIES.
- BENCHMARK ELEVATION =EAST QUARTER CORNER SECTION 7, T4N, R2W SLB&M (FOUND) ELEV. = 4234.28.

ENGINEER:



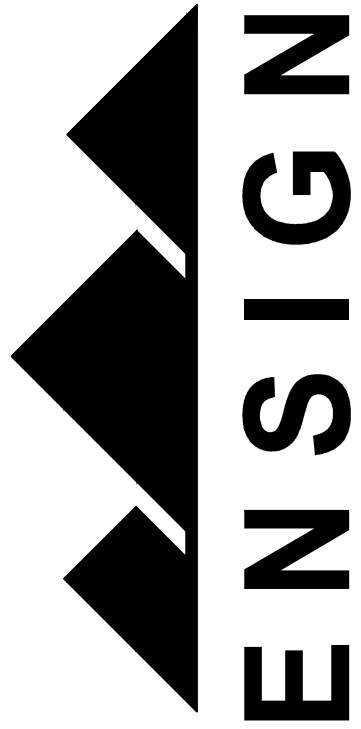
LAYTON  
1486 W. Hillfield Rd. Ste 204  
Layton UT 84041  
Phone: 801.547.1100  
Fax: 801.593.6315

WWW.ENSIGNENG.COM

SALT LAKE CITY  
Phone: 801.255.0029  
TUCOLELL  
Phone: 435.843.3690  
CEDAR CITY  
Phone: 435.865.1463  
RICHFIELD  
Phone: 435.896.2983

CRIDDLE FARMS NORTH SUBDIVISION PHASE 2

FOR REVIEW - JANUARY 11, 2019





# CRIDDLE FARMS NORTH PHASE 2 SUBDIVISION

LOCATED IN THE NORTHEAST QUARTER  
OF SECTION 7  
TOWNSHIP 4 NORTH RANGE 2 WEST  
SALT LAKE BASE & MERIDIAN  
SYRACUSE CITY, DAVIS COUNTY, UTAH

Utility Easement in favor of West Point City and Syracuse City, Utah  
for the construction of Sanitary Sewer, Land Drain and Storm Drain  
for Criddle Farms North Subdivisions

A 40.00 Foot Permanent Utilities Easement, situate in the Northeast Quarter of Section 7, Township 4 North, Range 2 West, Salt Lake Base and Meridian, said parcel also located in West Point, Utah, more particularly described as follows:

Beginning at the end of a Boundary Line Agreement in the office of the Davis County Recorder, recorded on \_\_\_\_\_, as Entry No. \_\_\_\_\_, in Book \_\_\_\_\_, at Page \_\_\_\_\_, said point also being on the Westerly Boundary Line of a proposed subdivision to be known as Criddle Farms North Phase 2 Subdivision at a point North 0°14'20" East 1318.68 feet along the section line to the Northeast Corner of Criddle Farms South Subdivision and South 89°58'20" West 1325.66 feet along the north line of Criddle Farms South Subdivision to and along the north line of Criddle Farms North Phase 1 Subdivision, to and along the south line to the Southwest Corner of a proposed subdivision to be known as Criddle Farms North Phase 2 Subdivision and North 0°14'33" East 153.75 feet along the west line of a proposed subdivision to be known as Criddle Farms North Phase 2 Subdivision from the East Quarter Corner of Section 7, Township 4 North, Range 2 West, Salt Lake Base and Meridian, and running:  
thence South 0°14'33" West 40.00 feet along the westerly line of said proposed subdivision to be known as Criddle Farms North Phase 2 Subdivision;  
thence South 89°17'45" West 648.18 feet;  
thence South 84°29'55" West 243.34 feet;  
thence South 89°58'20" West 404.44 feet to the east line of 4500 West Street, said point being North 0°14'47" East 1398.67 feet along the quarter section line and North 89°58'20" East 33.00 feet from the Center of Section 7, Township 4 North, Range 2 West, Salt Lake Base and Meridian;  
thence North 0°14'47" East 40.00 feet along the east line of 4500 West Street;  
thence North 89°58'20" East 402.35 feet;  
thence North 84°29'55" East 243.03 feet to a point on a line defined in the aforementioned Boundary Line Agreement;  
thence North 89°17'45" East 648.58 feet along the line defined in the said Boundary Line Agreement to the westerly line of said proposed subdivision to be known as Criddle Farms North Phase 2 Subdivision, being the point of beginning.

Contains 51,755 square feet, 1.189 acres.

See EXHIBIT "A"

Temporary Construction Easement in favor of West Point City and Syracuse City, Utah  
for the construction of Sanitary Sewer, Land Drain and Storm Drain  
for Criddle Farms North Subdivisions

A Temporary Construction easement running adjacent to and north of the above described 40 foot Permanent Utility Easement, more particularly described as follows:

Beginning at a point on the line of a Boundary Line Agreement in the office of the Davis County Recorder, recorded on \_\_\_\_\_, as Entry No. \_\_\_\_\_, in Book \_\_\_\_\_, at Page \_\_\_\_\_, said point being North 0°14'20" East 1318.68 feet along the section line to the Northeast Corner of Criddle Farms South Subdivision and South 89°58'20" West 1325.66 feet along the north line of Criddle Farms South Subdivision to and along the north line of Criddle Farms North Phase 1 Subdivision, to and along the south line to the Southwest Corner of a proposed subdivision to be known as Criddle Farms North Phase 2 Subdivision and North 0°14'33" East 153.75 feet along the west line of a proposed subdivision to be known as Criddle Farms North Phase 2 Subdivision to the end of the aforementioned Boundary Line Agreement and South 89°17'45" West 648.58 feet along the line defined in the Boundary Line Agreement from the East Quarter Corner of Section 7, Township 4 North, Range 2 West, Salt Lake Base and Meridian, and running:

thence South 84°29'55" West 243.03 feet;  
thence South 89°58'20" West 402.35 feet to the east line of 4500 West Street;  
thence North 0°14'47" East 15.58 feet along the east line of 4500 West Street to the Point of Beginning of the aforementioned Boundary Line Agreement, said point being North 0°14'47" East 1453.86 feet along the quarter section line and North 89°17'45" East 33.00 feet from the Center of Section 7, Township 4 North, Range 2 West, Salt Lake Base and Meridian;  
thence North 89°17'45" East 644.24 feet along the line defined in the aforementioned Boundary Line Agreement to the point of beginning.

Contains 9,681 square feet, 0.222 acres.

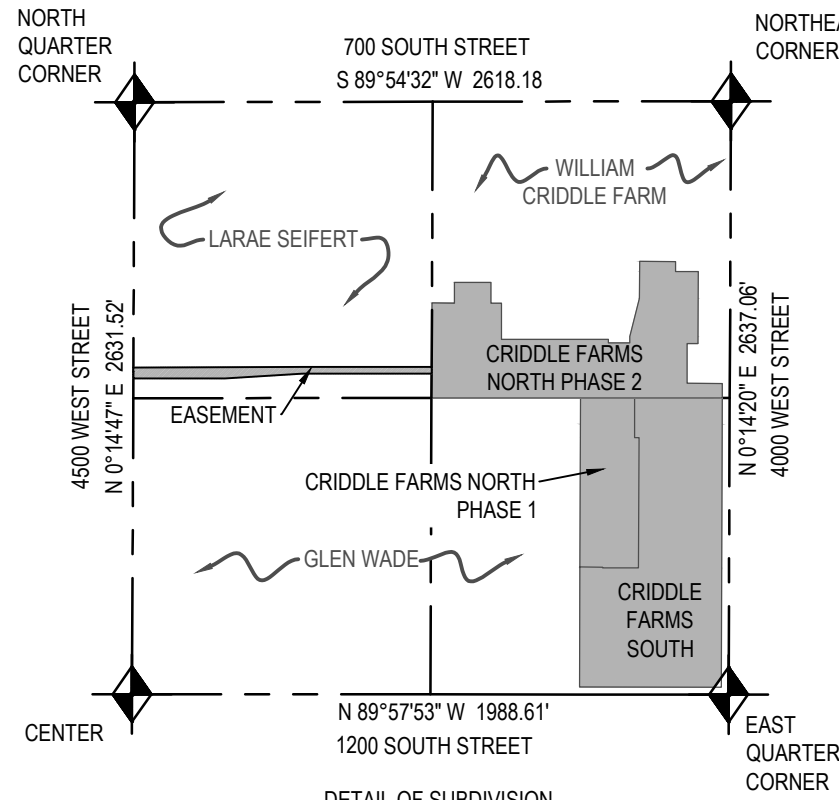
See EXHIBIT "B" on Page 4

Boundary Line Agreement Legal Description  
Glen L. and Nancy B. Wade and  
Larae D. Seifert, Trustee and Susan L. Elbrader, Trustee

Beginning at a point on the line east line of 4500 West Street, said point currently marked by a steel "T" post and being North 0°14'47" East 1453.86 feet along the quarter section line and the center of 4500 West Street and North 89°17'45" East 33.00 feet from the Center of Section 7, Township 4 North, Range 2 West, Salt Lake Base and Meridian;

Thence North 89°17'45" East 1292.82 feet to the west line of a proposed subdivision to be known as Criddle Farms North Phase 2 Subdivision, being the end.

Tax ID Parcel No. 12-046-0036 Glen L. Wade and Nancy B. Wade  
Tax ID Parcel No. 12-046-0054 Larae D. Seifert, Trustee  
Tax ID Parcel No. 12-046-0055 Susan L. Elbrader, Trustee



## SURVEYOR'S CERTIFICATE

I, MICHAEL B. HERBST, do hereby certify that I am a Licensed Land Surveyor, and that I hold certificate No. 5046930, as prescribed under laws of the State of Utah. I further certify that by authority of the Owners, I have made a survey of the tract of land shown on this plat and described below, and have subdivided said tract of land into lots and streets, hereafter to be known as CRIDDLE FARMS NORTH PHASE 2 SUBDIVISION, and that the same has been correctly surveyed and staked on the ground as shown on this plat. I further certify that all lots meet frontage width and area requirements of the applicable zoning ordinances.

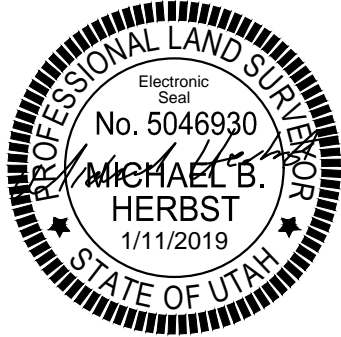
## BOUNDARY DESCRIPTION

Criddle Farms North Phase 2 Subdivision Description

Beginning at the Northeast Corner of Criddle Farms South Subdivision, said point being North 0°14'20" East 1318.68 feet along the quarter section line to the extension of the north line of Criddle Farms South Subdivision and South 89°58'20" West 33.00 feet from the East Quarter Corner of Section 7, Township 4 North, Range 2 West, Salt Lake Base and Meridian, and running;

Thence South 89°58'20" West 1292.66 feet along the north line of Criddle Farms South Subdivision to and along the North Line of Criddle Farms North Phase 1 Subdivision and beyond, to the Southwest Corner of the Northeast Quarter of the Northeast Quarter of said Section 7;  
Thence North 0°14'33" East 422.11 feet along the west line of the Northeast Quarter of the Northeast Quarter of Section 7;  
Thence South 89°45'27" East 100.00 feet;  
Thence North 0°14'33" East 91.77 feet;  
Thence South 89°45'27" East 161.38 feet;  
Thence South 0°01'40" East 91.75 feet;  
Thence North 89°58'20" East 47.13 feet;  
Thence South 0°01'40" East 160.00 feet;  
Thence North 89°58'20" East 475.00 feet;  
Thence South 0°01'40" East 15.89 feet;  
Thence North 89°58'20" East 95.00 feet;  
Thence North 0°01'40" West 27.39 feet;  
Thence North 14°23'37" East 175.81 feet;  
Thence North 0°14'20" East 164.00 feet;  
Thence South 89°45'40" East 160.00 feet;  
Thence South 0°14'20" West 43.71 feet;  
Thence South 89°45'40" East 100.00 feet;  
Thence South 0°14'20" West 320.00 feet;  
Thence North 89°45'40" West 47.54 feet;  
Thence South 0°14'20" West 177.35 feet;  
Thence South 89°45'40" East 157.54 feet to the west line of 4000 West Street;  
Thence South 0°14'20" West 63.88 feet along the west line of 4000 West Street to the point of beginning.

Contains 459,657 square feet, 10.552 acres, 33 lots and 2 Parcels.



Date 1/11/2019  
Michael B. Herbst  
License no. 5046930

## OWNER'S DEDICATION

Known all men by these presents that I, the undersigned owner of the above described tract of land, having caused same to be subdivided, hereafter known as the

## CRIDDLE FARMS NORTH PHASE 2 SUBDIVISION

do hereby dedicate for perpetual use of the public all parcels of land shown on this plat as intended for Public use and also warrant and defend the City harmless against any easements or other encumbrances on the dedicated streets which will interfere with the City's use, maintenance and operation of the streets. Parcel A and Parcel B are to be owned and maintained by the Criddle Farms Home Owners Association.

In witness whereof I have hereunto set my hand this \_\_\_\_\_ day of \_\_\_\_\_ A.D., 20\_\_\_\_.

D.R. HORTON, INC., a Delaware Corporation  
By: Boyd A. Martin  
Its: Vice President and President of the Division

## CORPORATE ACKNOWLEDGMENT

STATE OF UTAH  
County of Davis

On the \_\_\_\_\_ day of \_\_\_\_\_ A.D., 20\_\_\_\_, \_\_\_\_\_, Boyd A. Martin, personally appeared before me, the undersigned Notary Public, in and for said County of \_\_\_\_\_, in the State of Utah, who after being duly sworn, acknowledged to me that He is the VICE PRESIDENT AND PRESIDENT OF THE DIVISION of D.R. HORTON, INC., A DELAWARE CORPORATION, a Corporation and that He signed the Owner's Dedication freely and voluntarily for and in behalf of said Corporation for the purposes therein mentioned and acknowledged to me that said Corporation executed the same.

MY COMMISSION EXPIRES: \_\_\_\_\_  
RESIDING IN \_\_\_\_\_ COUNTY.

## CRIDDLE FARMS NORTH PHASE 2 SUBDIVISION

LOCATED IN THE NORTHEAST QUARTER  
OF SECTION 7  
TOWNSHIP 4 NORTH RANGE 2 WEST  
SALT LAKE BASE & MERIDIAN  
SYRACUSE CITY, DAVIS COUNTY, UTAH

## DAVIS COUNTY RECORDER

ENTRY NO. \_\_\_\_\_ FEE  
PAID \_\_\_\_\_ FILED FOR RECORD AND  
RECORDED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_ 20\_\_\_\_  
AT \_\_\_\_\_ IN BOOK \_\_\_\_\_ OF OFFICIAL RECORDS  
PAGE \_\_\_\_\_

DAVIS COUNTY RECORDER

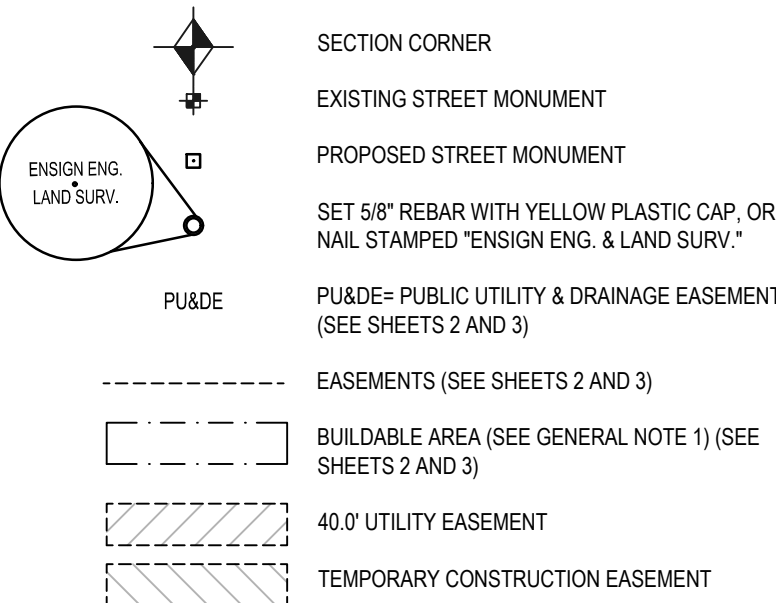
BY \_\_\_\_\_  
DEPUTY RECORDER

## GENERAL NOTES:

- PROPERTY IS ZONED R-3.
  - FRONT YARD SETBACK IS 25'
  - REAR YARD SETBACK IS 20'
  - SIDE YARD SETBACK IS 8'
  - CORNER LOT SIDE YARD SETBACK IS 20' STREET SIDE
- ALL PUBLIC UTILITY AND DRAINAGE EASEMENTS (PU & DE) ARE 10' FRONT, 5' SIDE AND 10' REAR UNLESS OTHERWISE NOTED HEREON.
- STREET TREES ARE REQUIRED ALONG 4000 WEST STREET, ONE EVERY 50 FEET.
- OWNERSHIP AND MAINTENANCE OF PARCEL A AND PARCEL B IS THE CRIDDLE FARMS HOME OWNERS ASSOCIATION.

NOTE:  
UTILITIES SHALL HAVE THE RIGHT TO INSTALL, MAINTAIN, AND OPERATE THEIR EQUIPMENT ABOVE AND BELOW GROUND AND ALL OTHER RELATED FACILITIES WITHIN THE PUBLIC UTILITY EASEMENTS IDENTIFIED ON THIS PLAT MAP AS MAY BE NECESSARY OR DESIRABLE IN PROVIDING UTILITY SERVICES WITHIN AND WITHOUT THE LOTS IDENTIFIED HEREIN, INCLUDING THE RIGHT OF ACCESS TO SUCH FACILITIES AND THE RIGHT TO REQUIRE REMOVAL OF ANY OBSTRUCTIONS INCLUDING STRUCTURES, TREES AND VEGETATION THAT MAY BE PLACED WITHIN THE P.U.E. THE UTILITY MAY REQUIRE THE LOT OWNER TO REMOVE ALL STRUCTURES WITHIN THE P.U.E. AT THE LOT OWNER'S EXPENSE, OR THE UTILITY MAY REMOVE SUCH STRUCTURES AT THE LOT OWNER'S EXPENSE. AT NO TIME MAY ANY PERMANENT STRUCTURES BE PLACED WITHIN THE P.U.E. OR ANY OTHER OBSTRUCTION WHICH INTERFERES WITH THE USE OF THE P.U.E. WITHOUT THE PRIOR WRITTEN APPROVAL OF THE UTILITIES WITH FACILITIES IN THE P.U.E.

## LEGEND



## UTILITY COMPANY APPROVAL

ROCKY MOUNTAIN POWER	DATE
DOMINION ENERGY	DATE
COMCAST CABLE	DATE
CENTURY LINK	DATE

## CITY ATTORNEY'S APPROVAL

APPROVED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_,  
BY THE SYRACUSE CITY ATTORNEY

SYRACUSE CITY ATTORNEY

## PLANNING COMMISSION APPROVAL

APPROVED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_,  
BY THE CITY PLANNING COMMISSION APPROVAL

CHAIRMAN, SYRACUSE CITY PLANNING COMMISSION

## CITY ENGINEER'S APPROVAL

APPROVED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_,  
BY THE SYRACUSE CITY ENGINEER

SYRACUSE CITY ENGINEER

## CITY COUNCIL APPROVAL

APPROVED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_,  
BY THE SYRACUSE CITY COUNCIL

CITY RECORDER CITY MAYOR

## SHEET 1 OF 2

PROJECT NUMBER : L2101G

MANAGER : KRUSSELL

DRAWN BY : JMOSS

CHECKED BY : KRUSSELL

DATE : 1/11/19

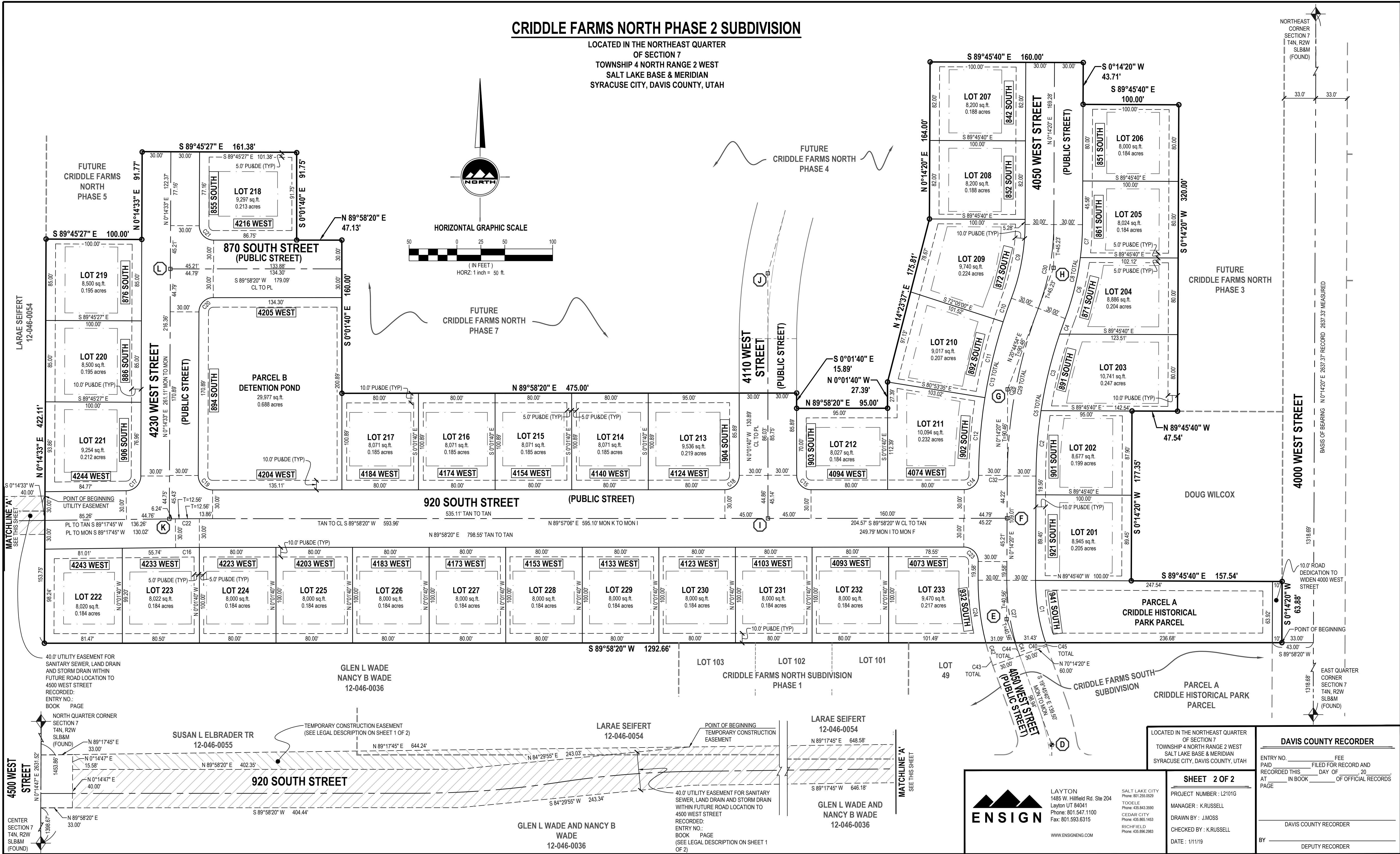


CRIDDLE FARMS NORTH PHASE 2 SUBDIVISION

LOCATED IN THE NORTHEAST QUARTER  
OF SECTION 7  
TOWNSHIP 4 NORTH RANGE 2 WEST  
SALT LAKE BASE & MERIDIAN  
SYRACUSE CITY, DAVIS COUNTY, UTAH



HORIZONTAL GRAPHIC SCALE  
(IN FEET)  
HORZ: 1 inch = 50 ft.



LAYTON  
1485 W. Hillfield Rd. Ste 204  
Layton UT 84041  
Phone: 801.547.1100  
Fax: 801.593.6315  
WWW.ENSGNENG.COM

SALT LAKE CITY  
Phone: 801.255.0929  
TOOELE  
Phone: 435.843.3990  
CEDAR CITY  
Phone: 435.865.1493  
RICHFIELD  
Phone: 435.996.2883

LOCATED IN THE NORTHEAST QUARTER  
OF SECTION 7  
TOWNSHIP 4 NORTH RANGE 2 WEST  
SALT LAKE BASE & MERIDIAN  
SYRACUSE CITY, DAVIS COUNTY, UTAH

SHEET 2 OF 2

PROJECT NUMBER: L2101G  
MANAGER: K. KRUSSELL  
DRAWN BY: J. MOSS  
CHECKED BY: K. KRUSSELL  
DATE: 1/11/19

DAVIS COUNTY RECORDER

ENTRY NO. \_\_\_\_\_ FEE  
PAID \_\_\_\_\_ FILED FOR RECORD AND  
RECORDED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_  
AT \_\_\_\_\_ IN BOOK \_\_\_\_\_ OF OFFICIAL RECORDS  
PAGE \_\_\_\_\_

DAVIS COUNTY RECORDER

BY \_\_\_\_\_ DEPUTY RECORDER

GENERAL NOTES

- ALL CONSTRUCTION MUST STRICTLY FOLLOW THE STANDARDS AND SPECIFICATIONS SET FORTH BY: THE DESIGN ENGINEER, LOCAL AGENCY JURISDICTION, APWA (2012 EDITION), AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.). THE ORDER LISTED ABOVE IS ARRANGED BY SENIORITY. THE LATEST EDITION OF ALL STANDARDS AND SPECIFICATIONS MUST BE ADHERED TO. IF A CONSTRUCTION PRACTICE IS NOT SPECIFIED BY ANY OF THE LISTED SOURCES, CONTRACTOR MUST CONTACT DESIGN ENGINEER FOR DIRECTION.
- CONTRACTOR TO STRICTLY FOLLOW THE MOST CURRENT COPY OF THE SOILS REPORT FOR THIS PROJECT. ALL GRADING INCLUDING BUT NOT LIMITED TO CUT, FILL, COMPACTION, ASPHALT SECTION, SUBBASE, TRENCH EXCAVATION/BACKFILL, SITE GRUBBING, AND FOOTINGS MUST BE COORDINATED DIRECTLY WITH SOILS REPORT.
- CONTRACTOR MUST VERIFY ALL EXISTING CONDITIONS BEFORE BIDDING, AND BRING UP ANY QUESTIONS BEFORE SUBMITTING BID.
- CONTRACTOR SHALL PROVIDE A CONSTRUCTION SCHEDULE IN ACCORDANCE WITH THE CITY, STATE, OR COUNTY REGULATIONS FOR WORKING IN THE PUBLIC WAY.
- CONTRACTOR SHALL BE RESPONSIBLE FOR DUST CONTROL ACCORDING TO GOVERNING AGENCY STANDARDS. WET DOWN DRY MATERIALS AND RUBBISH TO PREVENT BLOWING.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ADJACENT SURFACE IMPROVEMENTS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY SETTLEMENT OF OR DAMAGE TO EXISTING UTILITIES.
- THE CONTRACTOR IS RESPONSIBLE TO FURNISH ALL MATERIALS TO COMPLETE THE PROJECT.
- UNLESS OTHERWISE NOTED, ALL ON-GRADE CONCRETE WILL BE PLACED ON A MINIMUM 4" GRAVEL BASE OVER A WELL COMPACTED (85% DENSITY PER ASTM D-1557) SUB GRADE.
- ALL EXPOSED SURFACES WILL HAVE A TEXTURED FINISH, RUBBED, OR BROOMED. ANY "PLASTERING" OF NEW CONCRETE WILL BE DONE WHILE IT IS STILL "GREEN".
- PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THAT ALL REQUIRED PERMITS AND APPROVALS HAVE BEEN OBTAINED. NO CONSTRUCTION OR FABRICATION SHALL BEGIN UNTIL THE CONTRACTOR HAS RECEIVED AND THOROUGHLY REVIEWED ALL PLANS AND OTHER DOCUMENTS APPROVED BY ALL OF THE PERMITTING AUTHORITIES.
- THE LOCATIONS OF UNDERGROUND FACILITIES SHOWN ON THESE PLANS ARE BASED ON FIELD SURVEYS AND LOCAL UTILITY COMPANY RECORDS. IT SHALL BE THE CONTRACTOR'S FULL RESPONSIBILITY TO CONTACT THE VARIOUS UTILITY COMPANIES TO LOCATE THEIR FACILITIES PRIOR TO PROCEEDING WITH CONSTRUCTION. NO ADDITIONAL COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR DAMAGE AND REPAIR TO THESE FACILITIES CAUSED BY HIS WORK FORCE. CONTRACTOR SHALL START INSTALLATION AT LOW POINT OF ALL NEW GRAVITY UTILITY LINES.
- ALL DIMENSIONS, GRADES, AND UTILITY DESIGN SHOWN ON THE PLANS SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOTIFY THE ENGINEER IF ANY DISCREPANCIES EXIST. PRIOR TO PROCEEDINGS WITH CONSTRUCTION FOR NECESSARY PLAN OR GRADE CHANGES, NO EXTRA COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR WORK HAVING TO BE REDONE DUE TO THE DIMENSIONS OR GRADES SHOWN INCORRECTLY ON THESE PLANS, IF SUCH NOTIFICATION HAS NOT BEEN GIVEN.
- NO CHANGE IN DESIGN LOCATION OR GRADE WILL BE MADE BY THE CONTRACTOR WITHOUT THE WRITTEN APPROVAL OF THE PROJECT ENGINEER.
- NATURAL VEGETATION AND SOIL COVER SHALL NOT BE DISTURBED PRIOR TO ACTUAL CONSTRUCTION OF A REQUIRED FACILITY OR IMPROVEMENT. MASS CLEARING OF THE SITE IN ANTICIPATION OF CONSTRUCTION SHALL BE AVOIDED.
- CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, MAINTAINING, OR RESTORING ALL MONUMENTS AND MONUMENT REFERENCE MARKS WITHIN THE PROJECT SITE. CONTACT THE CITY OR COUNTY SURVEYOR FOR MONUMENT LOCATIONS AND CONSTRUCTION DETAILS.
- CONTRACTOR TO LAYOUT AND POTHOLE FOR ALL POTENTIAL CONFLICTS WITH UTILITY LINES ON- OR OFF-SITE AS REQUIRED PRIOR TO ANY CONSTRUCTION, AND THE CONTRACTOR WILL VERIFY DEPTHS OF UTILITIES IN THE FIELD BY POTHOLING A MINIMUM OF 300 FEET AHEAD OF PIPELINE CONSTRUCTION TO AVOID CONFLICTS WITH DESIGNED PIPELINE GRADE AND ALIGNMENT. IF A CONFLICT ARISES RESULTING FROM THE CONTRACTOR'S NEGLIGENCE TO POTHOLE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO RESOLVE THE CONFLICT WITHOUT ADDITIONAL COST OR CLAIM TO THE OWNER OR ENGINEER.
- ANY AREA OUTSIDE THE LIMIT OF WORK THAT IS DISTURBED SHALL BE RESTORED TO ITS ORIGINAL CONDITION AT NO COST TO OWNER.
- CONSULT ALL OF THE DRAWINGS AND SPECIFICATIONS FOR COORDINATION REQUIREMENTS BEFORE COMMENCING CONSTRUCTION.
- AT ALL LOCATIONS WHERE EXISTING PAVEMENT ABUTS NEW CONSTRUCTION, THE EDGE OF THE EXISTING PAVEMENT SHALL BE SAWCUT TO A CLEAN, SMOOTH EDGE.
- ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE MOST RECENT, ADOPTED EDITION OF ADA ACCESSIBILITY GUIDELINES. ALL TRUNCATED DOME PLATES SHOULD ALIGN IN ACCORDANCE WITH ADA REQUIREMENTS.
- CONTRACTOR SHALL, AT THE TIME OF BIDDING AND THROUGHOUT THE PERIOD OF THE CONTRACT, BE LICENSED IN THE STATE OF UTAH AND SHALL BE BONDBLE FOR AN AMOUNT REQUIRED BY THE OWNER.
- CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE ALL WATER, POWER, SANITARY FACILITIES AND TELEPHONE SERVICES AS REQUIRED FOR THE CONTRACTOR'S USE DURING CONSTRUCTION.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ADEQUATELY SCHEDULING INSPECTION AND TESTING OF ALL FACILITIES CONSTRUCTED UNDER THIS CONTRACT. ALL TESTING SHALL CONFORM TO THE REGULATORY AGENCY'S STANDARD SPECIFICATIONS. ALL RE-TESTING AND/OR RE-INSPECTION SHALL BE PAID FOR BY THE CONTRACTOR.
- IF EXISTING IMPROVEMENTS NEED TO BE DISTURBED AND/OR REMOVED FOR THE PROPER PLACEMENT OF IMPROVEMENTS TO BE CONSTRUCTED BY THESE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING IMPROVEMENTS FROM DAMAGE. COST OF REPLACING OR REPAIRING EXISTING IMPROVEMENTS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEMS REQUIRING REMOVAL AND/OR REPLACEMENT. THERE WILL BE NO EXTRA COST DUE TO THE CONTRACTOR FOR REPLACING OR REPAIRING EXISTING IMPROVEMENTS.
- WHENEVER EXISTING FACILITIES ARE REMOVED, DAMAGED, BROKEN, OR CUT IN THE INSTALLATION OF THE WORK COVERED BY THESE PLANS OR SPECIFICATIONS, SAID FACILITIES SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE WITH MATERIALS EQUAL TO OR BETTER THAN THE MATERIALS USED IN THE ORIGINAL EXISTING FACILITIES. THE FINISHED PRODUCT SHALL BE SUBJECT TO THE APPROVAL OF THE OWNER, THE ENGINEER, AND THE RESPECTIVE REGULATORY AGENCY.
- CONTRACTOR SHALL MAINTAIN A NEATLY MARKED SET OF FULL-SIZE RECORD DRAWINGS SHOWING THE FINAL LOCATION AND LAYOUT OF ALL STRUCTURES AND OTHER FACILITIES. RECORD DRAWINGS SHALL REFLECT CHANGE ORDERS, ACCOMMODATIONS, AND ADJUSTMENTS TO ALL IMPROVEMENTS CONSTRUCTED. WHERE NECESSARY, SUPPLEMENTAL DRAWINGS SHALL BE PREPARED AND SUBMITTED BY THE CONTRACTOR. PRIOR TO ACCEPTANCE OF THE PROJECT, THE CONTRACTOR SHALL DELIVER TO THE ENGINEER ONE SET OF NEATLY MARKED RECORD DRAWINGS SHOWING THE INFORMATION REQUIRED ABOVE. RECORD DRAWINGS SHALL BE REVIEWED AND THE COMPLETE RECORD DRAWING SET SHALL BE CURRENT WITH ALL CHANGES AND DEVIATIONS REDLINED AS A PRECONDITION TO THE FINAL PROGRESS PAYMENT APPROVAL AND/OR FINAL ACCEPTANCE.
- WHERE THE PLANS OR SPECIFICATIONS DESCRIBE PORTIONS OF THE WORK IN GENERAL TERMS BUT NOT IN COMPLETE DETAIL, IT IS UNDERSTOOD THAT ONLY THE BEST GENERAL PRACTICE IS TO PREVAIL AND THAT ONLY MATERIALS AND WORKMANSHIP OF THE FIRST QUALITY ARE TO BE USED.
- ALL EXISTING GATES AND FENCES TO REMAIN UNLESS OTHERWISE NOTED ON PLANS. PROTECT ALL GATES AND FENCES FROM DAMAGE.
- ALL EXISTING TREES ARE TO REMAIN UNLESS OTHERWISE NOTED ON PLANS. PROTECT ALL TREES FROM DAMAGE.
- ASPHALT MIX DESIGN MUST BE SUBMITTED AND APPROVED BY THE CITY ENGINEER PRIOR TO THE GRADING OF ASPHALT WITHIN THE CITY RIGHT-OF-WAY AND/OR UDOT IF WITHIN A UDOT RIGHT-OF-WAY.
- CONTRACTORS ARE RESPONSIBLE FOR ALL OSHA REQUIREMENTS ON THE PROJECT SITE.
- A UPDES [UTAH POLLUTANT DISCHARGE ELIMINATION SYSTEM] PERMIT IS REQUIRED FOR ALL CONSTRUCTION ACTIVITIES 1 ACRE OR MORE AS WELL AS A STORM WATER POLLUTION PREVENTION PLAN.

DEMOLITION NOTES

- EXISTING UTILITY INFORMATION SHOWN IS FOR INFORMATIONAL PURPOSES ONLY. IT IS DERIVED FROM ON-SITE SURVEY AND MAY NOT BE LOCATED CORRECTLY AND IS NOT ALL INCLUSIVE. CONTRACTOR SHALL FIELD LOCATE ALL UTILITIES WITHIN THE PROJECT LIMITS BEFORE BEGINNING DEMOLITION/CONSTRUCTION.
- THERE MAY BE BURIED UTILITIES WITHIN THE LIMITS OF DISTURBANCE THAT ARE NOT SHOWN ON THE PLANS DUE TO LACK OF MAPPING OR RECORD INFORMATION. CONTRACTOR SHALL NOTIFY THE ENGINEER WHEN UNEXPECTED UTILITIES ARE DISCOVERED.
- THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR LOCATING AND PROTECTING FROM DAMAGE ALL EXISTING UTILITIES AND IMPROVEMENTS WHETHER OR NOT SHOWN ON THESE PLANS. THE FACILITIES AND IMPROVEMENTS ARE BELIEVED TO BE CORRECTLY SHOWN BUT THE CONTRACTOR IS REQUIRED TO SATISFY HIMSELF AS TO THE COMPLETENESS AND ACCURACY OF THE LOCATIONS. ANY CONTRACTOR PERFORMING WORK ON THIS PROJECT SHALL FAMILIARIZE HIMSELF WITH THE SITE AND SHALL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGE TO EXISTING FACILITIES RESULTING DIRECTLY, OR INDIRECTLY, FROM HIS OPERATIONS, WHETHER OR NOT SAID FACILITIES ARE SHOWN ON THESE PLANS.

UTILITY NOTES

- ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THESE CONTRACT DOCUMENTS, CITY AND STATE REQUIREMENTS AND THE MOST RECENT EDITIONS OF THE FOLLOWING: THE INTERNATIONAL PLUMBING CODE, UTAH DRINKING WATER REGULATIONS, APWA MANUAL OF STANDARD PLANS AND SPECIFICATIONS. THE CONTRACTOR IS REQUIRED TO ADHERE TO ALL OF THE ABOVE-MENTIONED DOCUMENTS UNLESS OTHERWISE NOTED AND APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL COORDINATE LOCATION OF NEW "DRY UTILITIES" WITH THE APPROPRIATE UTILITY COMPANY, INCLUDING BUT NOT LIMITED TO: TELEPHONE & INTERNET SERVICE, GAS SERVICE, CABLE, AND POWER.
- EXISTING UTILITIES HAVE BEEN SHOWN ON THE PLANS BASED ON ON-SITE SURVEY. PRIOR TO COMMENCING ANY WORK, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE EACH UTILITY COMPANY LOCATE, IN THE FIELD, THEIR MAIN AND SERVICE LINES. THE CONTRACTOR SHALL NOTIFY BLUE STAKES AT 1-800-862-4111 48 HOURS IN ADVANCE OF PERFORMING ANY EXCAVATION WORK. THE CONTRACTOR SHALL RECORD THE BLUE STAKES ORDER NUMBER AND FURNISH ORDER NUMBER TO OWNER AND ENGINEER PRIOR TO ANY EXCAVATION. IT WILL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO DIRECTLY CONTACT ANY OTHER UTILITY COMPANIES THAT ARE NOT MEMBERS OF BLUE STAKES. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO PROTECT ALL EXISTING UTILITIES SO THAT NO DAMAGE RESULTS TO THEM DURING THE PERFORMANCE OF THIS CONTRACT. ANY REPAIRS NECESSARY TO DAMAGED UTILITIES SHALL BE PAID FOR BY THE CONTRACTOR. THE CONTRACTOR SHALL BE REQUIRED TO COOPERATE WITH OTHER CONTRACTORS AND UTILITY COMPANIES INSTALLING NEW STRUCTURES, UTILITIES AND SERVICE TO THE PROJECT.
- CARE SHOULD BE TAKEN IN ALL EXCAVATIONS DUE TO POSSIBLE EXISTENCE OF UNRECORDED UTILITY LINES. EXCAVATION REQUIRED WITHIN PROXIMITY OF EXISTING UTILITY LINES SHALL BE DONE BY HAND. CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING UTILITY LINES OR STRUCTURES INCURRED DURING CONSTRUCTION OPERATIONS AT CONTRACTOR'S EXPENSE.
- TRENCH BACKFILL MATERIAL AND COMPACTION TESTS ARE TO BE TAKEN PER APWA STANDARD SPECIFICATIONS (2012 EDITION), SECTION 02320 - BACKFILLING TRENCHES, OR AS REQUIRED BY THE GEOTECHNICAL REPORT IF NATIVE MATERIALS ARE USED. NO NATIVE MATERIALS ARE ALLOWED IN THE PIPE ZONE. THE MAXIMUM LIFT FOR BACKFILLING EXCAVATIONS IS 8-INCHES.
- THE CONTRACTOR IS SOLELY RESPONSIBLE FOR CONFORMING TO LOCAL AND FEDERAL CODES GOVERNING SHORING AND BRACING OF EXCAVATIONS AND TRENCHES AND FOR THE PROTECTION OF WORKERS.
- THE CONTRACTOR IS REQUIRED TO KEEP ALL CONSTRUCTION ACTIVITIES WITHIN THE APPROVED PROJECT LIMITS. THIS INCLUDES, BUT IS NOT LIMITED TO VEHICLE AND EQUIPMENT STAGING, MATERIAL STORAGE AND LIMITS OF TRENCH EXCAVATION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN PERMISSION AND/OR EASEMENTS FROM THE APPROPRIATE GOVERNING ENTITY AND/OR INDIVIDUAL PROPERTY OWNER(S) FOR WORK OR STAGING OUTSIDE OF THE PROJECT LIMITS.
- THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE, CAUSED BY ANY CONDITION INCLUDING SETTLEMENT, TO EXISTING UTILITIES FROM WORK PERFORMED AT OR NEAR EXISTING UTILITIES. THE CONTRACTOR SHALL TAKE ALL MEASURES NECESSARY TO PROTECT ALL EXISTING PUBLIC AND PRIVATE ROADWAY AND UTILITY FACILITIES. DAMAGE TO EXISTING FACILITIES CAUSED BY THE CONTRACTOR MUST BE REPAIRED BY THE CONTRACTOR AT HIS/HER EXPENSE TO THE SATISFACTION OF THE OWNER OF SAID FACILITIES.
- ALL WATER LINE AND SEWER LINE INSTALLATION AND TESTING TO BE IN ACCORDANCE WITH LOCAL GOVERNING AGENCY'S STANDARDS AND SPECIFICATIONS.
- ALL MANHOLES, HYDRANTS, VALVES, CLEANOUT BOXES, CATCH BASINS, METERS, ETC. MUST BE RAISED OR LOWERED TO FINAL GRADE PER APWA (2012 EDITION) STANDARDS AND INSPECTOR REQUIREMENTS. CONCRETE COLLARS MUST BE CONSTRUCTED ON ALL MANHOLES, CLEANOUT BOXES, CATCH BASINS, AND VALVES PER APWA STANDARDS. ALL MANHOLE, CATCH BASIN, OR CLEANOUT BOX CONNECTIONS MUST BE MADE WITH THE PIPE CUT FLUSH WITH THE INSIDE OF THE BOX AND GROUTED OR SEALED.
- CONTRACTOR SHALL NOT ALLOW ANY GROUNDWATER OR DEBRIS TO ENTER THE NEW OR EXISTING PIPE DURING CONSTRUCTION.
- SILT AND DEBRIS ARE TO BE CLEANED OUT OF ALL STORM DRAIN BOXES. CATCH BASINS ARE TO BE MAINTAINED IN A CLEANED CONDITION AS NEEDED UNTIL AFTER THE FINAL BOMB RELEASE INSPECTION.
- CONTRACTOR SHALL CLEAN ASPHALT, TAR OR OTHER ADHESIVES OFF OF ALL MANHOLE LIDS AND INLET GRATES TO ALLOW ACCESS.
- EACH TRENCH SHALL BE EXCAVATED SO THAT THE PIPE CAN BE LAID TO THE ALIGNMENT AND GRADE AS REQUIRED. THE TRENCH WALL SHALL BE SO BRACED THAT THE WORKMEN MAY WORK SAFELY AND EFFICIENTLY. ALL TRENCHES SHALL BE DRAINED SO THE PIPE LAYING MAY TAKE PLACE IN DEWATERED CONDITIONS.
- CONTRACTOR SHALL PROVIDE AND MAINTAIN AT ALL TIMES AMPLE MEANS AND DEVICES WITH WHICH TO REMOVE PROMPTLY AND TO PROPERLY DISPOSE OF ALL WATER ENTERING THE TRENCH EXCAVATION.
- ALL SEWER LINES AND SEWER SERVICES SHALL HAVE A MINIMUM SEPARATION OF 10 FEET, CENTER TO CENTER, FROM THE WATER LINES. IF A 10 FOOT SEPARATION CAN NOT BE MAINTAINED, THE SEWER LINE AND WATER LINE SHALL BE LAID IN SEPARATE TRENCHES AND THERE SHALL BE A MINIMUM 18" VERTICAL SEPARATION BETWEEN THE PIPES.
- CONTRACTOR SHALL INSTALL THRUST BLOCKING AT ALL WATERLINE ANGLE POINTS AND TEES.
- ALL UNDERGROUND UTILITIES SHALL BE IN PLACE PRIOR TO INSTALLATION OF CURB, GUTTER, SIDEWALK AND STREET PAVING.
- CONTRACTOR SHALL INSTALL MAGNETIC LOCATING TAPE CONTINUOUSLY OVER ALL NONMETALLIC PIPE.
- ALL VALVES SHALL BE FLANGED ONTO TEES (INCLUDING FIRE HYDRANTS).

UTAH DEPARTMENT OF TRANSPORTATION (UDOT) NOTES

- UDOT RESERVES THE RIGHT, AT ITS OPTION, TO INSTALL A RAISED MEDIAN ISLAND OR RESTRICT THE ACCESS TO A RIGHT-OR RIGHT-OUT AT ANY TIME.
- WORK ON THE UDOT RIGHT-OF-WAY IS RESTRICTED FROM OCTOBER 15- APRIL 15.
- ROW WORK: WORK IS NOT ALLOWED ON THE RIGHT-OF-WAY DURING THE AMPM PEAK TRAFFIC HOURS (6:00 - 9:00 AM AND 3:30 - 6:00 PM). ADDITIONAL WORK RESTRICTIONS OR MODIFICATIONS MAY BE IMPOSED AT THE TIME OF THE ENCROACHMENT PERMIT.
- REPLACE ALL PAVEMENT MARKINGS IN KIND (TAPE WITH TAPE AND PAINT WITH PAINT). INSTALL ALL PAINT LINES WITH PERMANENT PAINT APPLICATION PER UDOT SPECIFICATION 02765. PAINT MUST HAVE AT LEAST 6 MONTHS LIFE AS DETERMINED BY UDOT'S PERMITS OFFICER.
- ALL NEW PAVEMENT WORDS, ARROWS AND SYMBOLS MARKING WITHIN THE RIGHT-OF-WAY SHALL BE PRE-FORMED THERMO PLASTIC. ALL LETTERS, ARROWS, AND SYMBOLS SHALL CONFORM WITH THE "STANDARD ALPHABET FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS" ADOPTED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- ALL SIGNS INSTALLED ON THE UDOT RIGHT-OF-WAY MUST BE HIGH INTENSITY GRADE (TYPE IX SHEETING) WITH A B3 SLIP BASE. INSTALL ALL SIGNS PER UDOT SN SERIES STANDARD DRAWINGS.
- BEFORE COMMENCING WORK ON THE STATE HIGHWAY, THE GENERAL CONTRACTOR IS REQUIRED TO OBTAIN AN ENCROACHMENT PERMIT FROM THE APPLICABLE REGION'S PERMITS OFFICE BEFORE WORKING WITHIN THE STATE RIGHT-OF-WAY.
- NOT USED.
- FOR ALL UTILITY TAPS (ROAD CUTS), USE FLOWABLE FILL PER UDOTS CURRENT MIX DESIGN (50-150 PSI) UDOT SPEC. 03575 (3 FOOT MINIMUM OR TO THE BOTTOM OF THE ASPHALT SECTION).
- ALL UTILITIES WITHIN THE PAVED SURFACE MUST BE BORED.
- FOR EXCAVATIONS OUTSIDE OF THE ROADWAY, BACK FILL WITH UDOT APPROVED GRANULAR BORROW AND ROAD BASE. COMPACTION PER UDOT SPEC. 2056 AND 2721.
- OWNER, DEVELOPER, AND/OR THE CONTRACTOR IS REQUIRED TO HIRE AN INDEPENDENT COMPANY FOR ALL TESTING WITHIN THE UDOT RIGHT-OF- WAY.
- OWNER, DEVELOPER, AND THE CONTRACTOR ARE RESPONSIBLE FOR ANY DAMAGE TO THE UDOT RIGHT-OF-WAY THAT MAY BE DIRECTLY OR INDIRECTLY CAUSED BY THE DEVELOPMENT ACTIVITY.
- TRAFFIC SIGNAL INSTALLATION OR MODIFICATION REQUIRES A SEPARATE WARRANTY BOND ONCE THE WORK HAS BEEN COMPLETED AND ACCEPTED. THE PERMITEE IS RESPONSIBLE FOR HIRING AN INDEPENDENT INSPECTION COMPANY TO PERFORM INSPECTION SERVICES FOR ALL SIGNAL WORK COMPLETED. FOR A LIST OF THE UDOT APPROVED CONTRACTORS AND CONSULTANTS CONTACT THE APPROPRIATE REGION'S TRAFFIC SIGNALS ENGINEER.
- PARTIAL CONCRETE PANEL REPLACEMENT IS NOT ALLOWED. WHEN PANELS ARE REMOVED, THE ENTIRE PANEL IS REQUIRED TO BE REPLACED PER UDOT STANDARDS, SPECIFICATIONS, AND STANDARD DRAWINGS.
- DOUBLE SAW CUT THE CONCRETE TO PREVENT THE SPALLING OF OTHER CONCRETE PANELS AND TO AVOID OVER CUTS. OVER CUTS AND SPALLS WILL REQUIRE FULL PANEL REPLACEMENT.
- ALL ABOVE GROUND FEATURES INCLUDING UTILITIES (POLES, FIRE HYDRANTS, BOXES, ETC.) MUST BE RELOCATED OUT OF THE AASHTO CLEAR ZONE OR A MINIMUM OF 18" BEHIND CURB.

GRADING AND DRAINAGE NOTES

- SITE GRADING SHALL BE PERFORMED IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AND THE RECOMMENDATIONS SET FORTH IN THE GEOTECHNICAL REPORT AND ALL RELATED ADDENDUMS.
- THE CONTRACTOR SHALL STRIP AND CLEAR THE TOPSOIL, MAJOR ROOTS AND ORGANIC MATERIAL FROM ALL PROPOSED BUILDING AND PAVEMENT AREAS PRIOR TO SITE GRADING. (THE TOPSOIL MAY BE STOCKPILED FOR LATER USE IN LANDSCAPED AREAS.)
- THE CONTRACTOR SHALL REMOVE ALL ORGANIC MATERIAL AND OTHER DELETERIOUS MATERIALS PRIOR TO PLACING GRADING FILL OR BASE COURSE. THE AREA SHOULD BE PROOF-ROLLED TO IDENTIFY ANY SOFT AREAS. WHERE SOFT AREAS ARE ENCOUNTERED, THE CONTRACTOR SHALL REMOVE THE SOIL AND REPLACE WITH COMPACTED FILL.
- ALL DEBRIS PILES AND BERMS SHOULD BE REMOVED AND HAULED AWAY FROM SITE OR USED AS GENERAL FILL IN LANDSCAPED AREAS.
- THE CONTRACTOR SHALL CONSTRUCT THE BUILDING PAD TO THESE DESIGN PLANS AS PART OF THE SITE GRADING CONTRACT, AND STRICTLY ADHERE TO THE SITE PREPARATION AND GRADING REQUIREMENTS OUTLINED IN THE GEOTECHNICAL REPORT.
- THE CONTRACTOR SHALL GRADE THE PROJECT SITE TO PROVIDE A SMOOTH TRANSITION BETWEEN NEW AND EXISTING ASPHALT, CURB AND GUTTER, AND ADJOINING SITE IMPROVEMENTS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGE AND DEBRIS ON ADJACENT STREETS WHEN EQUIPMENT IS TRAVELING THOSE STREETS.
- THE CONTRACTOR SHALL BE FAMILIAR WITH ALL CONDITIONS AND RECOMMENDATIONS OUTLINED IN THE GEOTECHNICAL REPORT AND TAKE ALL NECESSARY PRECAUTIONS AND RECOMMENDED PROCEDURES TO ASSURE SOUND GRADING PRACTICES.
- THE CONTRACTOR SHALL TAKE APPROPRIATE GRADING MEASURES TO DIRECT STORM SURFACE RUNOFF TOWARDS CATCH BASINS.
- THE LOCATIONS OF UNDERGROUND FACILITIES SHOWN ON THESE PLANS ARE BASED ON ON-SITE SURVEY. IT SHALL BE THE CONTRACTOR'S FULL RESPONSIBILITY TO CONTACT THE VARIOUS UTILITY COMPANIES TO LOCATE THEIR FACILITIES PRIOR TO PROCEEDING WITH CONSTRUCTION. NO ADDITIONAL COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR DAMAGE AND REPAIR TO THESE FACILITIES CAUSED BY HIS WORK FORCE.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PERFORM ALL NECESSARY CUTS AND FILLS WITHIN THE LIMITS OF THIS PROJECT AND THE RELATED OFF-SITE WORK, SO AS TO GENERATE THE DESIRED SUBGRADE, FINISH GRADES, AND SLOPES SHOWN.
- THE CONTRACTOR IS WARNED THAT AN EARTHWORK BALANCE WAS NOT NECESSARILY THE INTENT OF THIS PROJECT. ANY ADDITIONAL MATERIAL REQUIRED OR LEFTOVER MATERIAL FOLLOWING EARTHWORK OPERATIONS BECOMES THE RESPONSIBILITY OF THE CONTRACTOR.
- THE GRADING CONTRACTOR IS RESPONSIBLE TO COORDINATE WITH THE OWNER TO PROVIDE FOR THE REQUIREMENTS OF THE PROJECT STORM WATER POLLUTION PREVENTION PLAN (SWPPP) AND ASSOCIATED PERMIT. ALL CONTRACTOR ACTIVITIES 1 ACRE OR MORE IN SIZE ARE REQUIRED TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN.
- ALL CUT AND FILL SLOPES SHALL BE PROTECTED UNTIL EFFECTIVE EROSION CONTROL HAS BEEN ESTABLISHED.
- THE USE OF POTABLE WATER WITHOUT A SPECIAL PERMIT FOR BUILDING OR CONSTRUCTION PURPOSES INCLUDING CONSOLIDATION OF BACKFILL OR DUST CONTROL IS PROHIBITED. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FOR CONSTRUCTION WATER FROM GOVERNING AGENCY.
- THE CONTRACTOR SHALL MAINTAIN THE STREETS, SIDEWALKS, AND ALL OTHER PUBLIC RIGHT-OF-WAYS IN A CLEAN, SAFE AND USABLE CONDITION. ALL SPILLS OF SOIL, ROCK OR CONSTRUCTION DEBRIS SHALL BE PROMPTLY REMOVED FROM THE PUBLICLY-OWNED PROPERTY DURING CONSTRUCTION AND UPON COMPLETION OF THE PROJECT. ALL ADJACENT PROPERTY, PRIVATE OR PUBLIC, SHALL BE MAINTAINED IN A CLEAN, SAFE, AND USABLE CONDITION.

TRAFFIC CONTROL AND SAFETY NOTES

- TRAFFIC CONTROL AND STRIPING TO CONFORM TO THE CURRENT MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.).
- BARRICADING AND DETOURING SHALL BE IN CONFORMANCE WITH THE REQUIREMENTS OF THE CURRENT M.U.T.C.D.
- NO STREET SHALL BE CLOSED TO TRAFFIC WITHOUT WRITTEN PERMISSION FROM THE APPROPRIATE AGENCY, EXCEPT WHEN DIRECTED BY LAW ENFORCEMENT OR FIRE OFFICIALS.
- THE CONTRACTOR SHALL MAKE EVERY EFFORT TO PROVIDE FOR SMOOTH TRAFFIC FLOW AND SAFETY. ACCESS SHALL BE MAINTAINED FOR ALL PROPERTIES ADJACENT TO THE WORK.
- DETOURING OPERATIONS FOR A PERIOD OF SIX CONSECUTIVE CALENDAR DAYS, OR MORE, REQUIRE THE INSTALLATION OF TEMPORARY STREET STRIPING AND REMOVAL OF INTERFERING STRIPING BY SANDBLASTING. THE DETOURING STRIPING PLAN OR CONSTRUCTION TRAFFIC CONTROL PLAN MUST BE SUBMITTED TO THE CITY TRAFFIC ENGINEER FOR REVIEW AND APPROVAL.
- ALL TRAFFIC CONTROL DEVICES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE END OF THE WORK TO THE SATISFACTION OF THE CITY TRAFFIC ENGINEER.
- TRAFFIC CONTROL DEVICES (TCDs) SHALL REMAIN VISIBLE AND OPERATIONAL AT ALL TIMES.
- ALL PERMANENT TRAFFIC CONTROL DEVICES CALLED FOR HEREON SHALL BE IN PLACE AND IN FINAL POSITION PRIOR TO ALLOWING ANY PUBLIC TRAFFIC ONTO THE PORTIONS OF THE ROAD(S) BEING IMPROVED HEREUNDER, REGARDLESS OF THE STATUS OF COMPLETION OF PAVING OR OTHER OFF-SITE IMPROVEMENTS CALLED FOR BY THESE PLANS.
- THE CONTRACTOR SHALL PROVIDE BARRICADES, SIGNS, FLASHERS, OTHER EQUIPMENT AND FLAG PERSONS NECESSARY TO INSURE THE SAFETY OF WORKERS AND VISITORS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING UTAH TRANSIT AUTHORITY (UTA) IF THE CONSTRUCTION INTERRUPTS OR RELOCATES A BUS STOP OR HAS AN ADVERSE EFFECT ON BUS SERVICE ON THAT STREET TO ARRANGE FOR TEMPORARY RELOCATION OF STOP.

ABBREVIATIONS

APWA	AMERICAN PUBLIC WORKS ASSOCIATION	NG	NATURAL GROUND
AR	ACCESSIBLE ROUTE	NIC	NOT IN CONTRACT
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS	NO	NUMBER
AWWA	AMERICAN WATER WORKS ASSOCIATION	OC	ON CENTER
BOS	BOTTOM OF STEP	OCBW	ON CENTER EACH WAY
BVC	BEGIN VERTICAL CURVE	OHP	OVERHEAD POWER
C	CURVE	PC	POINT OF CURVATURE OR PRESSURE CLASS
CB	CATCH BASIN	PCC	POINT OF COMPOUND CURVATURE
CF	CURB FACE OR CUBIC FEET	PI	POINT OF INTERSECTION
CL	CENTER LINE	PIV	POST INDICATOR VALVE
CO	CLEAN OUT	PL	PROPERTY LINE
COMM	COMMUNICATION	PRC	POINT OF REVERSE CURVATURE
CONC	CONCRETE	PRO	PROPOSED
CONT	CONTINUOUS	PT	POINT OF TANGENCY
DIA	DIAMETER	PVC	POINT OF VERTICAL CURVATURE
DIP	DUCTILE IRON PIPE	PVI	POINT OF VERTICAL INTERSECTION
ELEC	ELECTRICAL	PVT	POINT OF VERTICAL TANGENCY
ELEV	ELEVATION	R	RADIUS
EOP	EDGE OF ASPHALT	RD	ROAD DRAIN
EOD	EDGE OF GRAVEL	ROW	RIGHT OF WAY
EVC	END OF VERTICAL CURVE	S	SLOPE
EW	EACH WAY	SAN SWR	SANITARY SEWER
EXIST	EXISTING	SD	STORM DRAIN
FF	FINISH FLOOR	SEC	SECONDARY
FG	FINISH GRADE	SS	SANITARY SEWER
FH	FIRE HYDRANT	STA	STATION
FL	FLOW LINE OR FLANGE	SW	SIDEWALK
GB	GRADE BREAK	SWL	SECONDARY WATER LINE
GF	GARAGE FLOOR	TBC	TOP BACK OF CURB
GV	GATE VALVE	TG	TOP OF GRATE
HC	HANDICAP	TOA	TOP OF ASPHALT
HP	HIGH POINT	TOC	TOP OF CONCRETE
IRR	IRRIGATION	TOF	TOP OF FOUNDATION
K	RATE OF VERTICAL CURVATURE	TOW	TOP OF WALL
LD	LAND DRAIN	TOS	TOP OF STEP
LP	LINEAR FEET	TYP	TYPICAL
LP	LOW POINT	VC	VERTICAL CURVE
MH	MANHOLE	WIV	WALL INDICATOR VALVE
MJ	MECHANICAL JOINT	WL	WATER LINE

NOTE: MAY CONTAIN ABBREVIATIONS THAT ARE NOT USED IN THIS PLAN SET.

LEGEND

	SECTION CORNER		EXISTING EDGE OF ASPHALT
	EXISTING MONUMENT		PROPOSED EDGE OF ASPHALT
	PROPOSED MONUMENT		EXISTING STRIPING
	EXISTING REBAR AND CAP		PROPOSED STRIPING
	SET ENSIGN REBAR AND CAP		EXISTING FENCE
	EXISTING WATER METER		PROPOSED FENCE
	PROPOSED WATER METER		EXISTING FLOW LINE
	EXISTING WATER MANHOLE		PROPOSED FLOW LINE
	PROPOSED WATER MANHOLE		GRADE BREAK
	EXISTING WATER BOX		EXISTING STORM DRAIN LINE
	EXISTING WATER VALVE		PROPOSED STORM DRAIN LINE
	PROPOSED WATER VALVE		ROOF DRAIN LINE
	EXISTING FIRE HYDRANT		CATCHMENTS
	PROPOSED FIRE HYDRANT		HIGHWATER LINE
	PROPOSED FIRE DEPARTMENT CONNECTION		EXISTING SANITARY SEWER
	EXISTING SECONDARY WATER VALVE		PROPOSED SANITARY SEWER LINE
	PROPOSED SECONDARY WATER VALVE		PROPOSED SAN. SWR. SERVICE LINE
	EXISTING IRRIGATION BOX		EXISTING LAND DRAIN LINE
	EXISTING IRRIGATION VALVE		PROPOSED LAND DRAIN LINE
	PROPOSED IRRIGATION VALVE		PROPOSED LAND DRAIN SERVICE LINE
	EXISTING SANITARY SEWER MANHOLE		EXISTING CULINARY WATER LINE
	PROPOSED SANITARY SEWER MANHOLE		PROPOSED CULINARY WATER LINE
	EXISTING SANITARY CLEAN OUT		PROPOSED CULINARY WATER SERVICE LINE
	EXISTING STORM DRAIN CLEAN OUT BOX		EXISTING SECONDARY WATER LINE
	PROPOSED STORM DRAIN CLEAN OUT BOX		PROPOSED SECONDARY WATER LINE
	EXISTING STORM DRAIN INLET BOX		PROPOSED SEC. WATER SERVICE LINE
	EXISTING STORM DRAIN CATCH BASIN		EXISTING IRRIGATION LINE
	PROPOSED STORM DRAIN CATCH BASIN		PROPOSED STORM IRRIGATION LINE
	EXISTING STORM DRAIN COMBO BOX		EXISTING OVERHEAD POWER LINE
	PROPOSED STORM DRAIN COMBO BOX		EXISTING ELECTRICAL LINE
	EXISTING STORM DRAIN CLEAN OUT		EXISTING GAS LINE
	EXISTING STORM DRAIN CULVERT		EXISTING TELEPHONE LINE
	PROPOSED STORM DRAIN CULVERT		ACCESSIBLE ROUTE
	TEMPORARY SAG INLET PROTECTION		SAW CUT LINE
	TEMPORARY IN-LINE INLET PROTECTION		STRAW WATTLE
	ROOF DRAIN		TEMPORARY BERM
	EXISTING ELECTRICAL MANHOLE		TEMPORARY SILT FENCE
	EXISTING ELECTRICAL BOX		LIMITS OF DISTURBANCE
	EXISTING TRANSFORMER		EXISTING WALL
	EXISTING UTILITY POLE		PROPOSED WALL
	EXISTING LIGHT		EXISTING CONTOURS
	PROPOSED LIGHT		PROPOSED CONTOURS
	EXISTING GAS METER		BUILDABLE AREA WITHIN SETBACKS
	EXISTING GAS MANHOLE		PUBLIC DRAINAGE EASEMENT
	EXISTING GAS VALVE		EXISTING ASPHALT TO BE REMOVED
	EXISTING TELEPHONE MANHOLE		PROPOSED ASPHALT
	EXISTING TELEPHONE BOX		EXISTING CURB AND GUTTER
	EXISTING TRAFFIC SIGNAL BOX		PROPOSED CURB AND GUTTER
	EXISTING CABLE BOX		PROPOSED REVERSE PAN CURB AND GUTTER
	EXISTING BOLLARD		TRANSITION TO REVERSE PAN CURB
	PROPOSED BOLLARD		CONCRETE TO BE REMOVED
	EXISTING SIGN		EXISTING CONCRETE
	PROPOSED SIGN		PROPOSED CONCRETE
	EXISTING SPOT ELEVATION		BUILDING TO BE REMOVED
	PROPOSED SPOT ELEVATION		EXISTING BUILDING
	EXISTING FLOW DIRECTION		PROPOSED BUILDING
	EXISTING TREE		
	DENSE VEGETATION		

NOTE: MAY CONTAIN SYMBOLS THAT ARE NOT USED IN THIS PLAN SET.



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Layton, UT 84041  
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SALT LAKE CITY  
Phone: 801.255.0529

TOOELE  
Phone: 435.843.3590

CEDAR CITY  
Phone: 435.865.1453

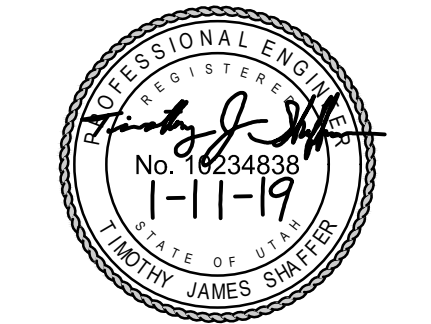
RICHFIELD  
Phone: 435.896.2983

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FOR:  
DR HORTON, INC.  
12351 S GATEWAY PARK PLACE SUITE D-100  
DRAPER, UTAH 84020

CONTACT:  
ADAM LOSER  
PHONE: 801-000-0000

CRIDDLE FARMS NORTH  
SUBDIVISION PHASE 2  
900 SOUTH TO 1200 SOUTH 4000 WEST STREET  
SYRACUSE, UTAH



NO.	DATE	REVISION	BY
1		FOR REVIEW	
2			
3			
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GENERAL NOTES

PROJECT NUMBER  
L2101F  
DRAWN BY  
J.MOSS  
PROJECT MANAGER  
K.RUSSELL

PRINT DATE  
1/11/19  
CHECKED BY  
C.PRESTON



811

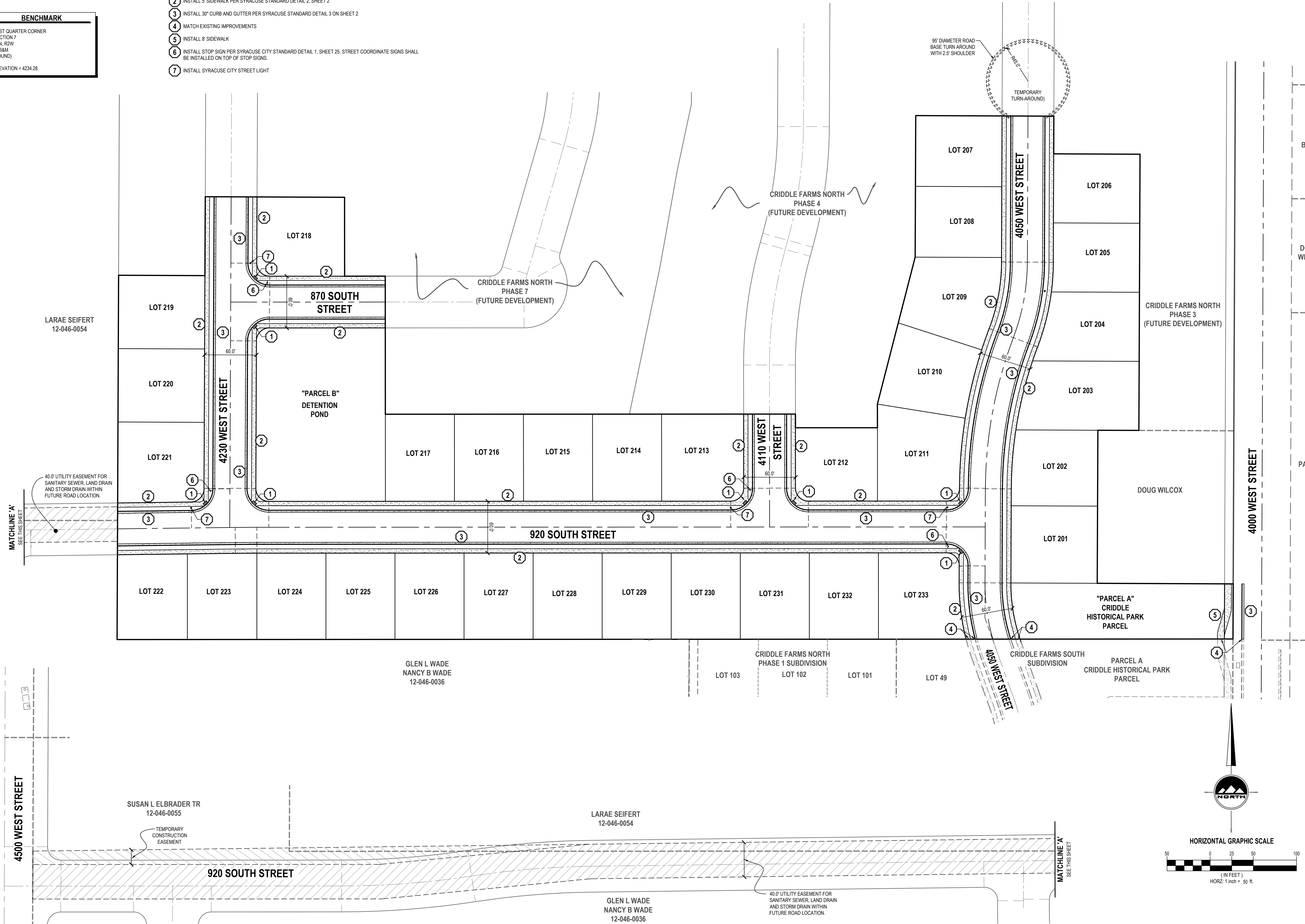
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PRIOR TO THE  
COMMENCEMENT OF ANY  
CONSTRUCTION.

BENCHMARK

EAST QUARTER CORNER  
SECTION 7  
T4N, R2W  
SLB&M  
(FOUND)  
  
ELEVATION = 4234.28

- SCOPE OF WORK:**  
PROVIDE, INSTALL AND/OR CONSTRUCT THE FOLLOWING PER THE SPECIFICATIONS GIVEN OR REFERENCED, THE DETAILS NOTED, AND/OR AS SHOWN ON THE CONSTRUCTION DRAWINGS:
1. INSTALL HANDICAP ACCESSIBLE RAMP PER DETAIL 4 SHEET A OF SYRACUSE CITY STANDARDS
  2. INSTALL 5' SIDEWALK PER SYRACUSE STANDARD DETAIL 2, SHEET 2
  3. INSTALL 30" CURB AND GUTTER PER SYRACUSE STANDARD DETAIL 3 ON SHEET 2
  4. MATCH EXISTING IMPROVEMENTS
  5. INSTALL 8' SIDEWALK
  6. INSTALL STOP SIGN PER SYRACUSE CITY STANDARD DETAIL 1, SHEET 25. STREET COORDINATE SIGNS SHALL BE INSTALLED ON TOP OF STOP SIGNS.
  7. INSTALL SYRACUSE CITY STREET LIGHT



EN SIGN

THE STANDARD IN ENGINEERING

LAYTON

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CRIDDLE FARMS NORTH  
SUBDIVISION PHASE 2  
900 SOUTH TO 1200 SOUTH 4000 WEST STREET  
SYRACUSE, UTAH

PROFESSIONAL ENGINEER

1-11-19

NO. 16234838

STATE OF UTAH

TRISTY JAMES SWINER

NO. DATE REASON BY

1 2 3 4 5 6 7 8

FOR REVIEW


SITE PLAN

PROJECT NUMBER L2101F PRINT DATE 1/11/19

DRAWN BY JMOSS CHECKED BY CPRESTON

PROJECT MANAGER K.RUSSELL

C-100

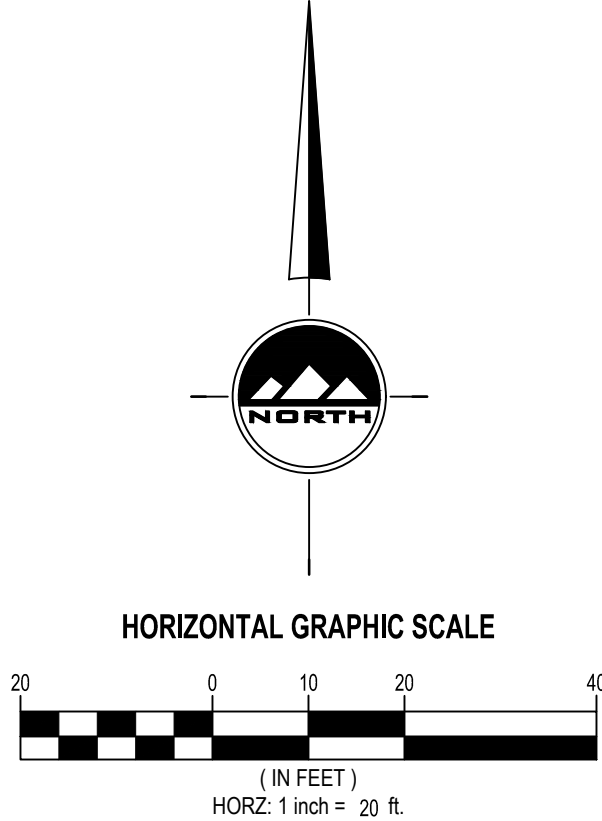
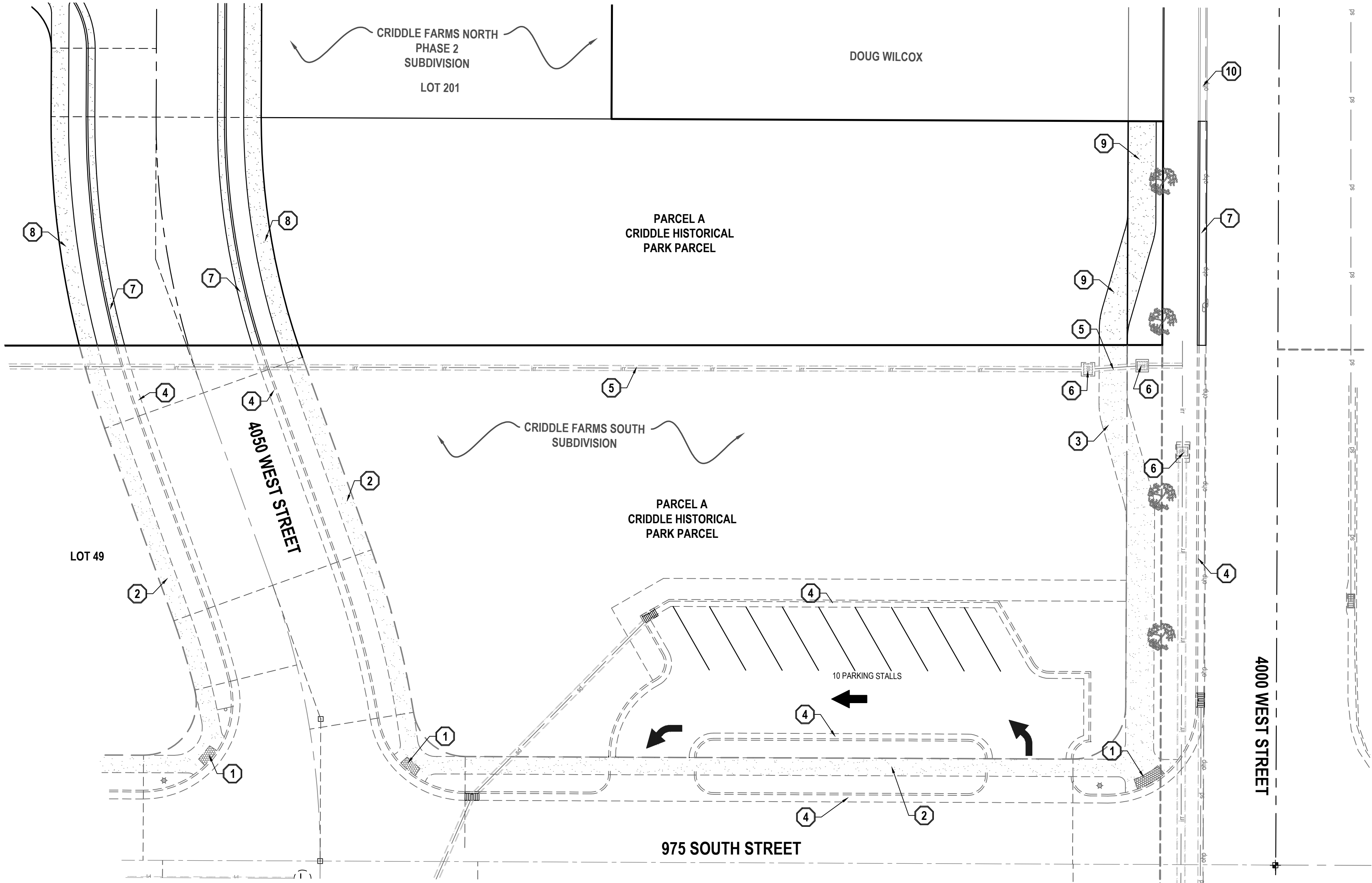


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**BENCHMARK**

EAST QUARTER CORNER  
SECTION 7  
T4N, R2W  
SLB&M  
(FOUND)  
  
ELEVATION = 4234.28

- KEYNOTES:
- 1 EXISTING HANDICAP ACCESSIBLE RAMP
  - 2 EXISTING 4' SIDEWALK
  - 3 EXISTING 8' SIDEWALK
  - 4 EXISTING 30" CURB AND GUTTER
  - 5 EXISTING 16" RCP IRRIGATION PIPE
  - 6 EXISTING IRRIGATION DIVERSION STRUCTURE
  - 7 PROPOSED CURB AND GUTTER
  - 8 PROPOSED 4' SIDEWALK
  - 9 PROPOSED 8' SIDEWALK
  - 10 FUTURE CURB AND GUTTER





**ENSIGN**  
THE STANDARD IN ENGINEERING

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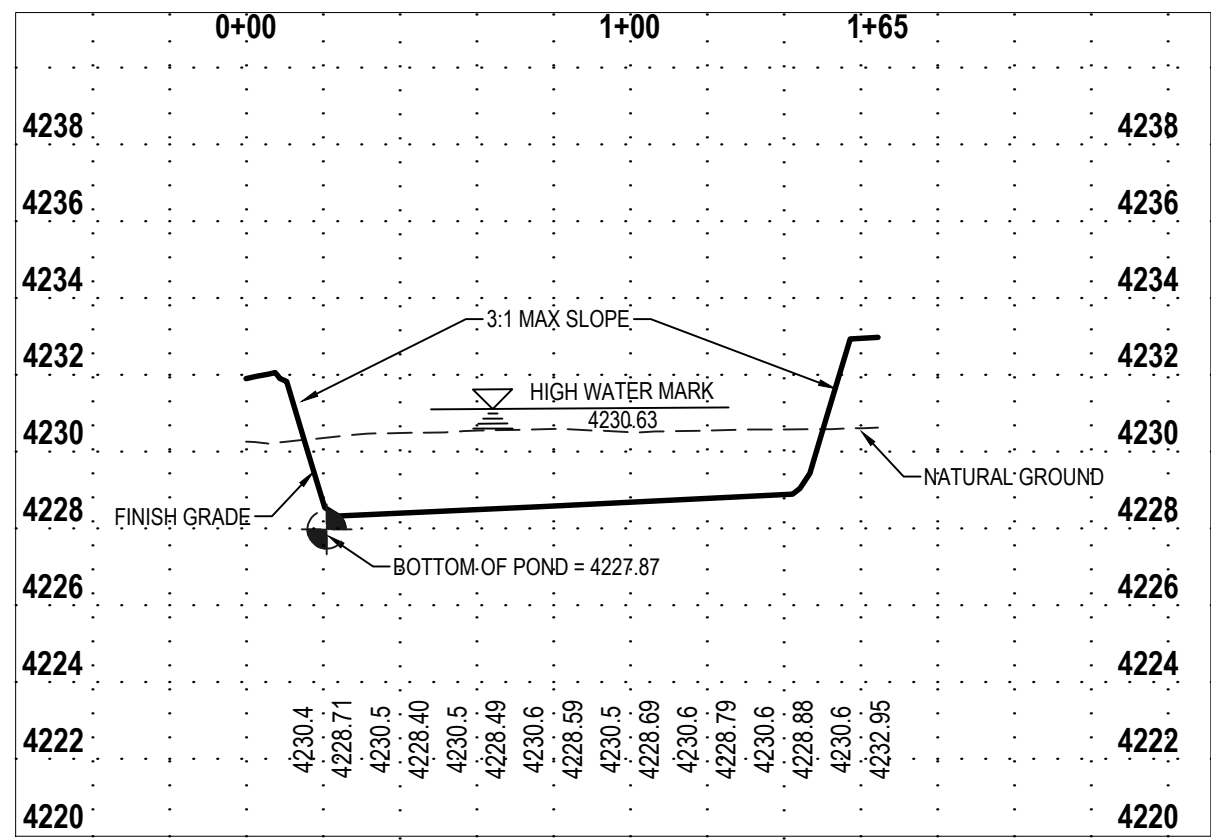
**HISTORICAL PARK  
SITE/LANDSCAPE PLAN**

PROJECT NUMBER L2101F	PRINT DATE 1/11/19
DRAWN BY J.MOSS	CHECKED BY C.PRESTON
PROJECT MANAGER K.RUSSELL	



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(FOUND)  
ELEVATION = 4234.28



**GENERAL NOTES**

- ALL WORK TO COMPLY WITH SYRACUSE CITY STANDARDS AND SPECIFICATIONS.
- ALL IMPROVEMENTS MUST COMPLY WITH ADA STANDARDS AND RECOMMENDATIONS.
- ALL WORK SHALL COMPLY WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL ENGINEER POSSIBLY INCLUDING, BUT NOT LIMITED TO, REMOVAL OF UNCONSOLIDATED FILL, ORGANICS, AND DEBRIS, PLACEMENT OF SUBSURFACE DRAIN LINES AND GEOTEXTILE, AND OVEREXCAVATION OF UNSUITABLE BEARING MATERIALS AND PLACEMENT OF ACCEPTABLE FILL MATERIAL.
- THE CONTRACTOR SHALL BECOME FAMILIAR WITH THE EXISTING SOIL CONDITIONS.
- LANDSCAPED AREAS REQUIRE SUBGRADE TO BE MAINTAINED AT A SPECIFIC ELEVATION BELOW FINISHED GRADE AND REQUIRE SUBGRADE TO BE PROPERLY PREPARED AND SCARIFIED. SEE LANDSCAPE PLANS FOR ADDITIONAL INFORMATION.
- SLOPE ALL LANDSCAPED AREAS AWAY FROM BUILDING FOUNDATIONS TOWARD CURB AND GUTTER OR STORM DRAIN INLETS.
- EXISTING UNDERGROUND UTILITIES AND IMPROVEMENTS ARE SHOWN IN THEIR APPROXIMATE LOCATIONS BASED UPON RECORD INFORMATION AVAILABLE AT THE TIME OF PREPARATION OF THESE PLANS. LOCATIONS MAY NOT HAVE BEEN VERIFIED IN THE FIELD AND NO GUARANTEE IS MADE AS TO THE ACCURACY OR COMPLETENESS OF THE INFORMATION SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE EXISTENCE AND LOCATION OF THE UTILITIES SHOWN ON THESE PLANS OR INDICATED IN THE FIELD BY LOCATING SERVICES. ANY ADDITIONAL COSTS INCURRED AS A RESULT OF THE CONTRACTOR'S FAILURE TO VERIFY THE LOCATIONS OF EXISTING UTILITIES PRIOR TO THE BEGINNING OF CONSTRUCTION IN THEIR VICINITY SHALL BE BORNE BY THE CONTRACTOR AND ASSUMED INCLUDED IN THE CONTRACT. THE CONTRACTOR IS TO VERIFY ALL CONNECTION POINTS WITH THE EXISTING UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE CAUSED TO THE EXISTING UTILITIES AND UTILITY STRUCTURES THAT ARE TO REMAIN. IF CONFLICTS WITH EXISTING UTILITIES OCCUR, THE CONTRACTOR SHALL NOTIFY THE ENGINEER PRIOR TO CONSTRUCTION TO DETERMINE IF ANY FIELD ADJUSTMENTS SHOULD BE MADE.
- ALL STORM DRAIN INFRASTRUCTURE TO BE INSTALLED PER SYRACUSE CITY STANDARD PLANS AND SPECIFICATIONS.
- ENSURE MINIMUM COVER OVER ALL STORM DRAIN PIPES PER MANUFACTURER'S RECOMMENDATIONS. NOTIFY ENGINEER IF MINIMUM COVER CANNOT BE ATTAINED.
- THE CONTRACTOR SHALL ADJUST TO GRADE ALL EXISTING UTILITIES AS NEEDED PER SYRACUSE CITY STANDARDS AND SPECIFICATIONS.
- NOTIFY ENGINEER OF ANY DISCREPANCIES IN DESIGN OR STAKING BEFORE PLACING CONCRETE, ASPHALT, OR STORM DRAIN STRUCTURES OR PIPES.
- THE CONTRACTOR IS TO PROTECT AND PRESERVE ALL EXISTING IMPROVEMENTS, UTILITIES, AND SIGNS, ETC. UNLESS OTHERWISE NOTED ON THESE PLANS.

Pipe Design Basin A (10-year storm)  
Manning's N (ADS) 0.011  
Manning's N (RCP) 0.013

Pipe	Tributary Basins	Surface Flow (CFS)	Upstream Pipes	Pipe Flow (CFS)	Total Flow (CFS)	Pipe Slope	Diameter (IN)	Pipe Type	Full Flow Capacity (CFS)	% of Full-Flow Capacity
SD-41	41	3.401	none	0.000	3.401	0.40%	15	RCP	4.097	83.0%
SD-42	42	0.849	none	0.000	0.849	0.40%	15	RCP	4.097	20.7%
SD-43	none	0.000	SD-41 and SD-44	4.250	4.250	0.45%	15	RCP	4.345	97.8%
SD-44	43	0.509	none	0.000	0.509	0.40%	15	RCP	4.097	12.4%
SD-45	44	0.260	SD-43 and SD-46	4.759	5.019	0.40%	18	RCP	6.661	75.3%
SD-46	45	0.691	none	0.000	0.691	0.40%	15	RCP	4.097	16.9%
SD-47	46	2.508	SD-45 and SD-48	8.217	8.217	0.40%	24	RCP	14.346	57.3%
SD-48	47	1.585	none	0.000	1.585	0.40%	15	RCP	4.097	38.7%
SD-49	48	1.128	SD-47 and SD-51	9.802	10.930	0.40%	24	RCP	14.346	76.2%
SD-50	49	1.414	none	0.000	1.414	0.40%	15	RCP	4.097	34.5%
SD-51	50	1.503	SD-49 and SD-53	3.759	3.871	0.28%	18	RCP	5.573	69.5%
SD-52	51	0.708	none	0.000	0.708	0.40%	15	RCP	4.097	17.3%
SD-53	52	2.147	SD-51 and SD-55	3.363	3.363	0.28%	15	RCP	3.427	98.1%
SD-54	53	0.396	DET-A	0.000	0.396	1.00%	12	ADS	4.222	9.4%
SD-55	54	0.112	SD-53 and SD-56	3.759	3.871	0.28%	18	RCP	5.573	69.5%

\*Detained Flow: 0.15 cfs/acre

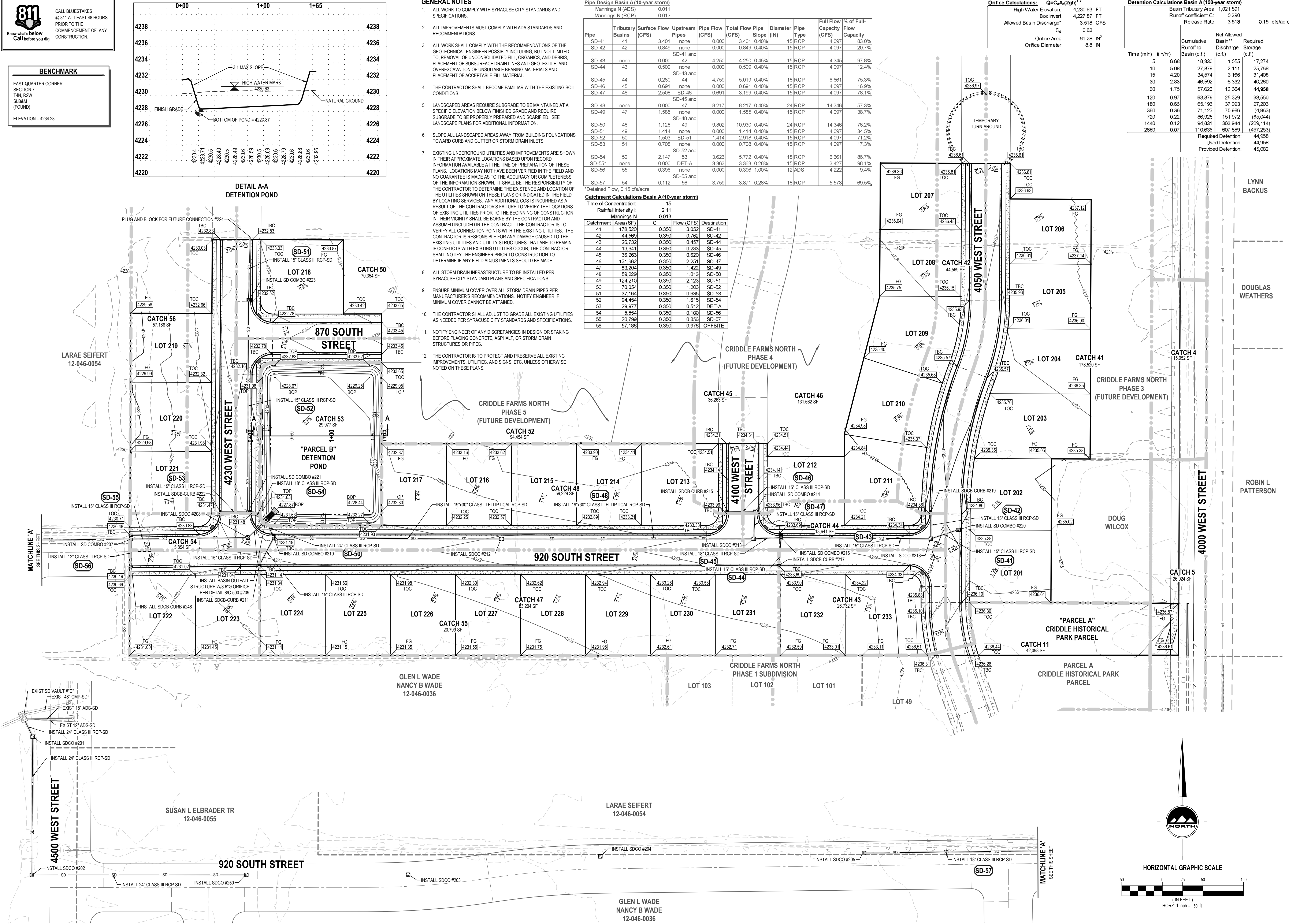
**Catchment Calculations Basin A (10-year storm)**

Time of Concentration:		15		
Rainfall Intensity I:		2.11		
Manning's N:		0.013		
Catchment	Area (SF)	C	Flow (CFS)	Destination
41	178,520	0.350	3,052	SD-41
42	44,969	0.350	0.762	SD-42
43	25,732	0.350	0.457	SD-44
44	13,641	0.350	0.233	SD-45
45	36,263	0.350	0.620	SD-46
46	131,962	0.350	2,251	SD-47
47	83,204	0.350	1,422	SD-49
48	59,229	0.350	1.013	SD-50
49	124,210	0.350	2,123	SD-51
50	70,354	0.350	1,203	SD-52
51	37,164	0.350	0.635	SD-53
52	94,454	0.350	1,815	SD-54
53	29,977	0.350	0.512	DET-A
54	5,854	0.350	0.100	SD-56
55	20,799	0.350	0.356	SD-57
56	57,188	0.350	0.978	OFFSITE

Orifice Calculations:  $Q = C_d A_o (2gh)^{1/2}$   
High Water Elevation 4,230.63 FT  
Box Invert 4,227.87 FT  
Allowed Basin Discharge\* 3.518 CFS  
 $C_d$  0.62  
Orifice Area 61.28 IN<sup>2</sup>  
Orifice Diameter 8.8 IN

**Detention Calculations Basin A (100-year storm)**

Basin Tributary Area		1,021.591		
Runoff coefficient C:		0.390		
Release Rate		3.518		
		0.15 cfs/acre		
Time (min)	i (in/hr)	Cumulative Runoff to Basin (c.f.)	Net Allowed Basin** Discharge (c.f.)	Required Storage (c.f.)
5	6.98	18,830	1,055	17,774
10	5.08	27,878	2,111	25,768
15	4.20	34,574	3,166	31,408
30	2.83	46,592	6,332	40,260
60	1.75	57,623	12,664	44,958
120	0.97	63,879	25,329	38,550
180	0.66	65,196	37,993	27,203
360	0.36	71,123	75,986	(4,863)
720	0.22	86,928	151,972	(65,044)
1440	0.12	94,831	303,944	(209,114)
2880	0.07	110,636	507,889	(497,253)
Required Detention:				44,958
Used Detention:				44,958
Provided Detention:				45,082



**EN SIGN**  
THE STANDARD IN ENGINEERING

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Phone: 801.255.0529

TOOELE  
Phone: 435.843.3590

CEDAR CITY  
Phone: 435.865.1453

RICHFIELD  
Phone: 435.896.2983

WWW.ENSIGNENG.COM

FOR:  
DR. HORTON, INC.  
12351 S GATEWAY PARK PLACE SUITE D-100  
DRAPER, UTAH 84020

CONTACT:  
ADAM LOSER  
PHONE: 801-000-0000

**CRIDDLE FARMS NORTH  
SUBDIVISION PHASE 2  
900 SOUTH TO 1200 SOUTH 4000 WEST STREET  
SYRACUSE, UTAH**



NO.	DATE	REASON	BY
1		FOR REVIEW	
2			
3			
4			
5			
6			
7			
8			

**GRADING AND  
DRAINAGE PLAN**

PROJECT NUMBER L2101F  
DRAWN BY JMOSS  
PROJECT MANAGER KRUSSSELL  
PRINT DATE 1/11/19  
CHECKED BY C.PRESTON

**C-200**



811

CALL BLUESTAKES  
@ 811 AT LEAST 48 HOURS  
PRIOR TO THE  
COMMENCEMENT OF ANY  
CONSTRUCTION.  
Call before you dig.

BENCHMARK

EAST QUARTER CORNER  
SECTION 7  
T4N, R2W  
SLB&M  
(FOUND)  
  
ELEVATION = 4234.28

- SCOPE OF WORK:  
PROVIDE, INSTALL AND/OR CONSTRUCT THE FOLLOWING PER THE SPECIFICATIONS GIVEN OR REFERENCED, THE DETAILS NOTED, AND/OR AS SHOWN ON THE CONSTRUCTION DRAWINGS.
- 1

3/4" CULINARY WATER LATERAL WITH 3/4" METER (TYP) PER SYRACUSE CITY STANDARD DETAIL 1, SHEET 8
- 2

4" SDR PVC SANITARY SEWER LATERAL (TYP), SLOPE 2% MINIMUM PER SYRACUSE CITY STANDARD DETAIL 1, SHEET 17
- 3

4" SDR PVC LAND DRAIN LATERAL (TYP), SLOPE 2% MINIMUM PER SYRACUSE CITY STANDARD DETAIL 1, SHEET 17
- 4

1 1/2" DUAL SECONDARY WATER LATERAL (TYP) WITH CURB STOP PER SYRACUSE CITY STANDARD DETAIL, SHEET 21
- 5

NOT USED
- 6

INSTALL 8" DR14 C900 "PURPLE" PVC SECONDARY WATERLINE PER SYRACUSE STANDARD DETAIL
- 7

INSTALL 8" DR14 C900 PVC CULINARY WATERLINE PER SYRACUSE CITY STANDARD
- 8

CAP AND BLOCK CULINARY WATERLINE.
- 9

CAP AND BLOCK SECONDARY WATERLINE FOR FUTURE CONNECTION.
- 10

INSTALL ECLIPSE 88 SAMPLING STATION PER SYRACUSE CITY STANDARD
- 11

INSTALL SYRACUSE CITY STREET LIGHT
- 12

LOCATE AND CONNECT TO EXISTING UTILITY
- 13

FIRE HYDRANT ASSEMBLY COMPLETE PER SYRACUSE CITY STANDARD DETAIL 2, SHEET 9
- 14

INSTALL AN AIR VAC ON CULINARY WATERLINE
- 15

INSTALL 2" DRAIN LINE FROM SECONDARY WATER MAIN TO STORM DRAIN CLEANOUT BOX

- GENERAL NOTES
1.

ALL WORK TO COMPLY WITH SYRACUSE CITY STANDARDS AND SPECIFICATIONS.
2.

EXISTING UNDERGROUND UTILITIES AND IMPROVEMENTS ARE SHOWN IN THEIR APPROXIMATE LOCATIONS BASED UPON RECORD INFORMATION AVAILABLE AT THE TIME OF PREPARATION OF THESE PLANS. LOCATIONS MAY NOT HAVE BEEN VERIFIED IN THE FIELD AND NO GUARANTEE IS MADE AS TO THE ACCURACY OR COMPLETENESS OF THE INFORMATION SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE EXISTENCE AND LOCATION OF THE UTILITIES SHOWN ON THESE PLANS OR INDICATED IN THE FIELD BY LOCATING SERVICES. ANY ADDITIONAL COSTS INCURRED AS A RESULT OF THE CONTRACTOR'S FAILURE TO VERIFY THE LOCATIONS OF EXISTING UTILITIES PRIOR TO THE BEGINNING OF CONSTRUCTION IN THEIR VICINITY SHALL BE BORNE BY THE CONTRACTOR AND ASSUMED INCLUDED IN THE CONTRACT. THE CONTRACTOR IS TO VERIFY ALL CONNECTION POINTS WITH THE EXISTING UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE CAUSED TO THE EXISTING UTILITIES AND UTILITY STRUCTURES THAT ARE TO REMAIN. IF CONFLICTS WITH EXISTING UTILITIES OCCUR, THE CONTRACTOR SHALL NOTIFY THE ENGINEER PRIOR TO CONSTRUCTION TO DETERMINE IF ANY FIELD ADJUSTMENTS SHOULD BE MADE.
3.

ALL SANITARY SEWER INFRASTRUCTURE TO BE INSTALLED PER SYRACUSE CITY STANDARD PLANS AND SPECIFICATIONS.
4.

ALL WATER INFRASTRUCTURE TO BE INSTALLED PER SYRACUSE CITY STANDARD PLANS AND SPECIFICATIONS.
5.

DEFLECT OR LOOP ALL WATERLINES TO AVOID CONFLICTS WITH OTHER UTILITIES PER SYRACUSE CITY STANDARDS AND SPECIFICATIONS.
6.

PROJECT SHALL COMPLY WITH ALL UTAH DIVISION OF DRINKING WATER RULES AND REGULATIONS INCLUDING, BUT NOT LIMITED TO, THOSE PERTAINING TO BACKFLOW PROTECTION AND CROSS CONNECTION PREVENTION.
7.

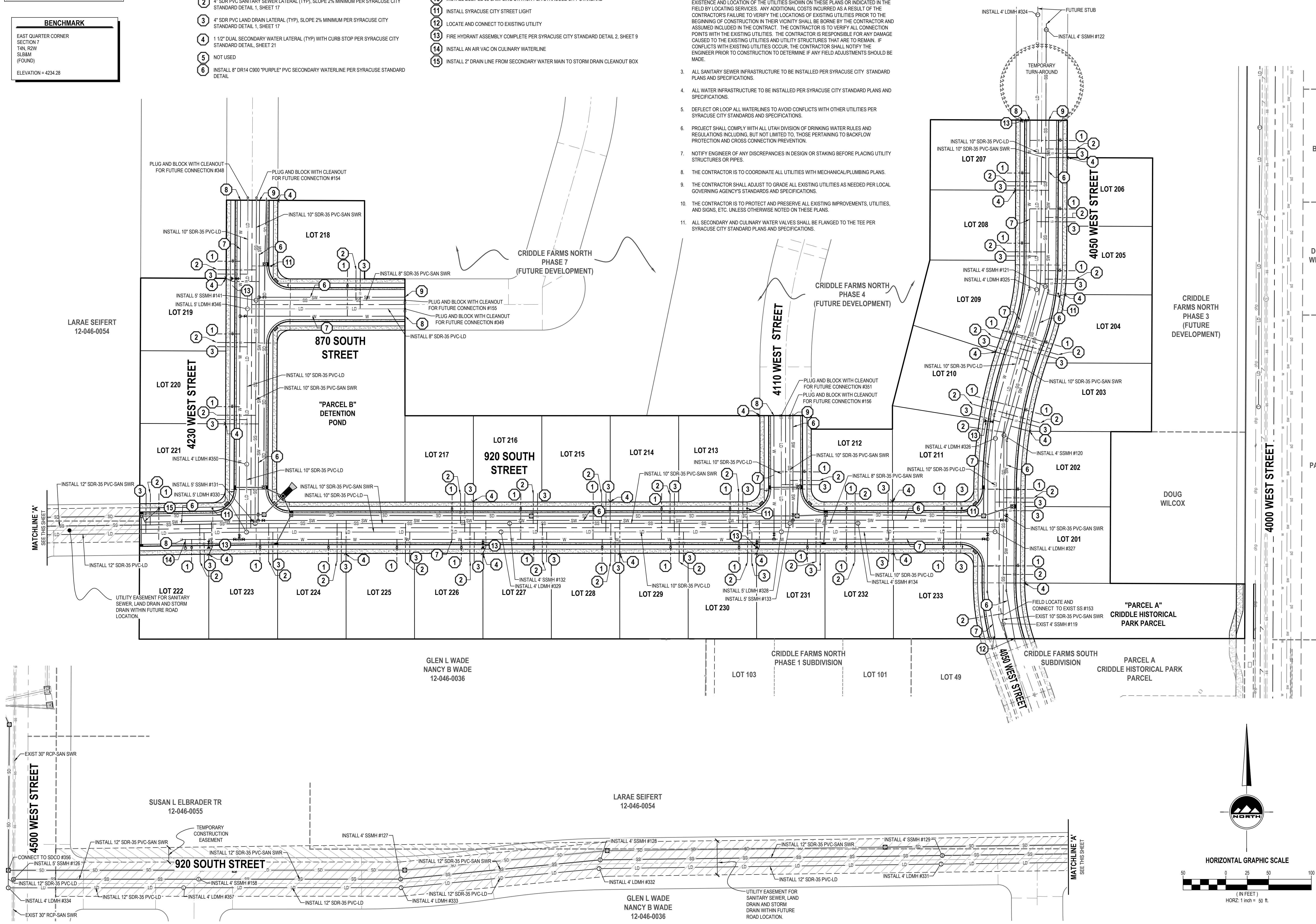
NOTIFY ENGINEER OF ANY DISCREPANCIES IN DESIGN OR STAKING BEFORE PLACING UTILITY STRUCTURES OR PIPES.
8.

THE CONTRACTOR IS TO COORDINATE ALL UTILITIES WITH MECHANICAL/PLUMBING PLANS.
9.

THE CONTRACTOR SHALL ADJUST TO GRADE ALL EXISTING UTILITIES AS NEEDED PER LOCAL GOVERNING AGENCY'S STANDARDS AND SPECIFICATIONS.
10.

THE CONTRACTOR IS TO PROTECT AND PRESERVE ALL EXISTING IMPROVEMENTS, UTILITIES, AND SIGNS, ETC. UNLESS OTHERWISE NOTED ON THESE PLANS.
11.

ALL SECONDARY AND CULINARY WATER VALVES SHALL BE FLANGED TO THE TEE PER SYRACUSE CITY STANDARD PLANS AND SPECIFICATIONS.



EN SIGN

THE STANDARD IN ENGINEERING

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CONTACT:

ADAM LOSER  
PHONE: 801-000-0000

CRIDDLE FARMS NORTH  
SUBDIVISION PHASE 2  
900 SOUTH TO 1200 SOUTH 4000 WEST STREET  
SYRACUSE, UTAH

PROFESSIONAL ENGINEER

1-11-19

THOMAS JAMES SWARTZ

NO. DATE REVISION BY

1 1/11/19 FOR REVIEW


UTILITY PLAN

PROJECT NUMBER L2101F PRINT DATE 1/11/19

DRAWN BY J.MOSS CHECKED BY C.PRESTON

PROJECT MANAGER K.RUSSELL

C-300



CALL BLUESTAKES  
@ 811 AT LEAST 48 HOURS  
PRIOR TO THE  
COMMENCEMENT OF ANY  
CONSTRUCTION.  
Know what's below.  
Call before you dig.

**BENCHMARK**

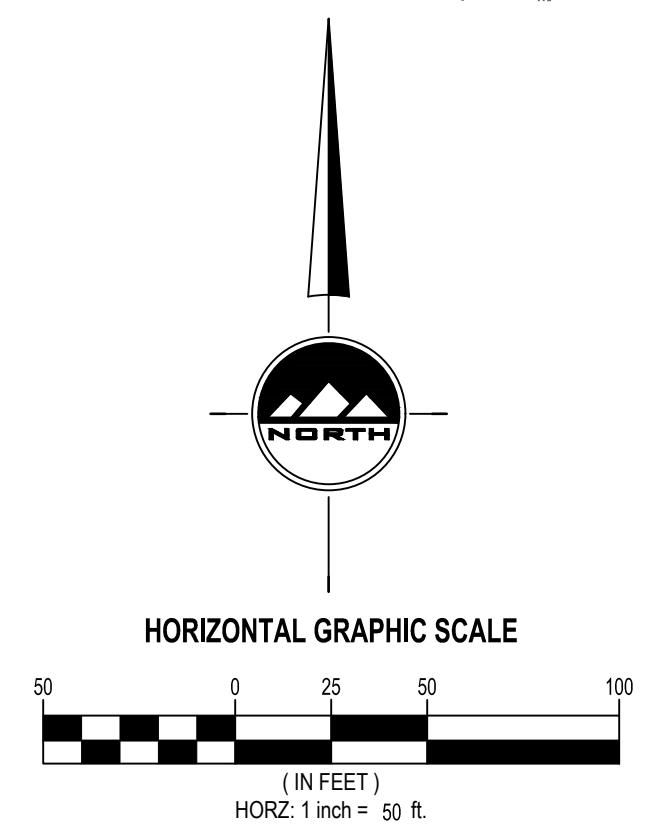
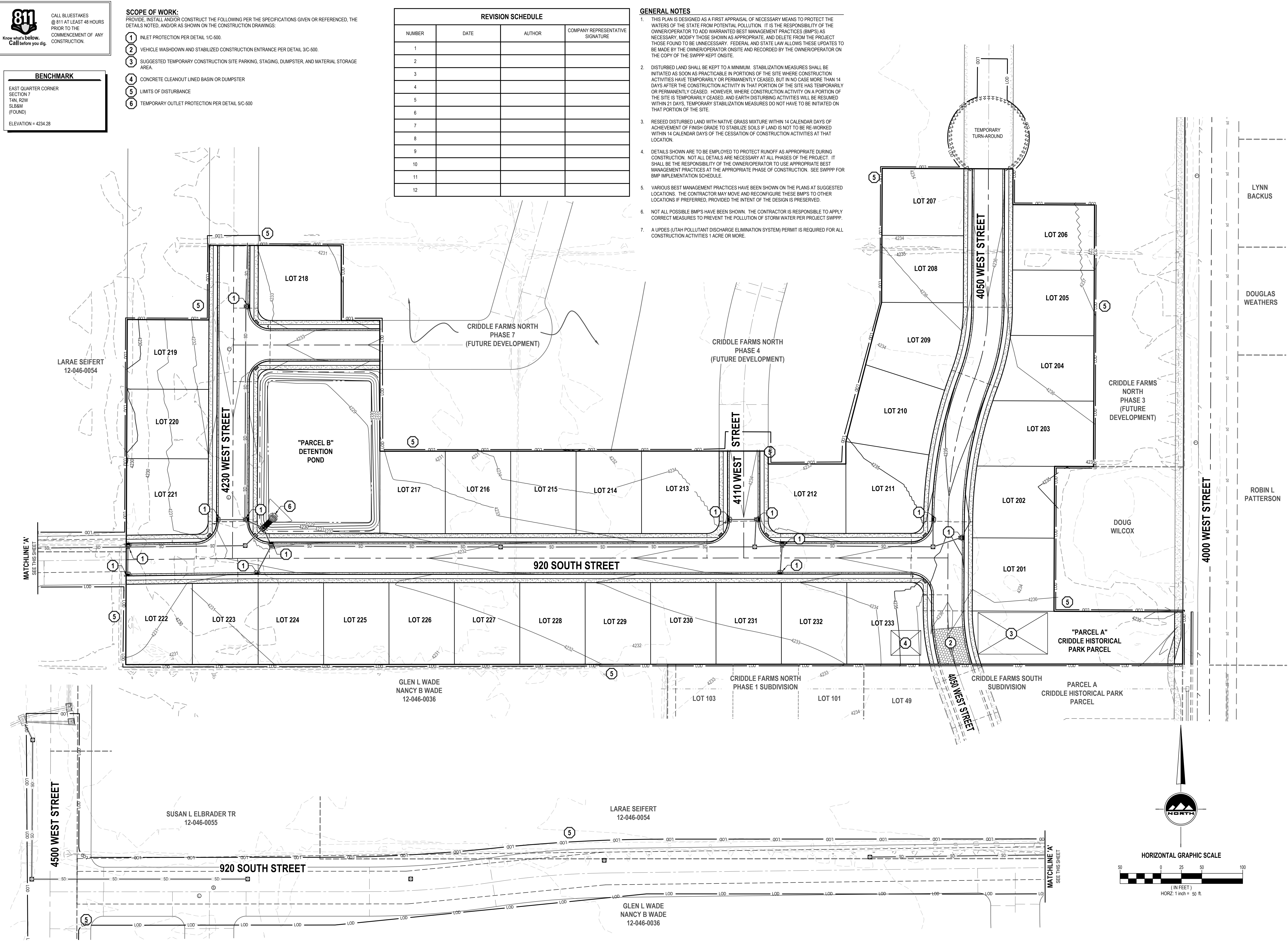
EAST QUARTER CORNER  
SECTION 7  
T4N, R2W  
SLB&M  
(FOUND)

ELEVATION = 4234.28

- SCOPE OF WORK:**  
PROVIDE, INSTALL AND/OR CONSTRUCT THE FOLLOWING PER THE SPECIFICATIONS GIVEN OR REFERENCED, THE DETAILS NOTED, AND/OR AS SHOWN ON THE CONSTRUCTION DRAWINGS:
- 1 INLET PROTECTION PER DETAIL 1/C-500.
  - 2 VEHICLE WASHDOWN AND STABILIZED CONSTRUCTION ENTRANCE PER DETAIL 3/C-500.
  - 3 SUGGESTED TEMPORARY CONSTRUCTION SITE PARKING, STAGING, DUMPSTER, AND MATERIAL STORAGE AREA.
  - 4 CONCRETE CLEANOUT LINED BASIN OR DUMPSTER
  - 5 LIMITS OF DISTURBANCE
  - 6 TEMPORARY OUTLET PROTECTION PER DETAIL 5/C-500

REVISION SCHEDULE			
NUMBER	DATE	AUTHOR	COMPANY REPRESENTATIVE SIGNATURE
1			
2			
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12			

- GENERAL NOTES**
- THIS PLAN IS DESIGNED AS A FIRST APPRAISAL OF NECESSARY MEANS TO PROTECT THE WATERS OF THE STATE FROM POTENTIAL POLLUTION. IT IS THE RESPONSIBILITY OF THE OWNER/OPERATOR TO ADD WARRANTED BEST MANAGEMENT PRACTICES (BMP'S) AS NECESSARY, MODIFY THOSE SHOWN AS APPROPRIATE, AND DELETE FROM THE PROJECT THOSE FOUND TO BE UNNECESSARY. FEDERAL AND STATE LAW ALLOWS THESE UPDATES TO BE MADE BY THE OWNER/OPERATOR ON-SITE AND RECORDED BY THE OWNER/OPERATOR ON THE COPY OF THE SWPPP KEPT ON-SITE.
  - DISTURBED LAND SHALL BE KEPT TO A MINIMUM. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED. HOWEVER, WHERE CONSTRUCTION ACTIVITY ON A PORTION OF THE SITE IS TEMPORARILY CEASED, AND EARTH DISTURBING ACTIVITIES WILL BE RESUMED WITHIN 21 DAYS, TEMPORARY STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE.
  - RESEED DISTURBED LAND WITH NATIVE GRASS MIXTURE WITHIN 14 CALENDAR DAYS OF ACHIEVEMENT OF FINISH GRADE TO STABILIZE SOILS IF LAND IS NOT TO BE RE-WORKED WITHIN 14 CALENDAR DAYS OF THE CESSATION OF CONSTRUCTION ACTIVITIES AT THAT LOCATION.
  - DETAILS SHOWN ARE TO BE EMPLOYED TO PROTECT RUNOFF AS APPROPRIATE DURING CONSTRUCTION. NOT ALL DETAILS ARE NECESSARY AT ALL PHASES OF THE PROJECT. IT SHALL BE THE RESPONSIBILITY OF THE OWNER/OPERATOR TO USE APPROPRIATE BEST MANAGEMENT PRACTICES AT THE APPROPRIATE PHASE OF CONSTRUCTION. SEE SWPPP FOR BMP IMPLEMENTATION SCHEDULE.
  - VARIOUS BEST MANAGEMENT PRACTICES HAVE BEEN SHOWN ON THE PLANS AT SUGGESTED LOCATIONS. THE CONTRACTOR MAY MOVE AND RECONFIGURE THESE BMP'S TO OTHER LOCATIONS IF PREFERRED, PROVIDED THE INTENT OF THE DESIGN IS PRESERVED.
  - NOT ALL POSSIBLE BMP'S HAVE BEEN SHOWN. THE CONTRACTOR IS RESPONSIBLE TO APPLY CORRECT MEASURES TO PREVENT THE POLLUTION OF STORM WATER PER PROJECT SWPPP.
  - A UPDES (UTAH POLLUTANT DISCHARGE ELIMINATION SYSTEM) PERMIT IS REQUIRED FOR ALL CONSTRUCTION ACTIVITIES 1 ACRE OR MORE.





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THE STANDARD IN ENGINEERING

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ADAM LOSER  
PHONE: 801-000-0000

**CRIDDLE FARMS NORTH  
SUBDIVISION PHASE 2**

900 SOUTH TO 1200 SOUTH 4000 WEST STREET  
SYRACUSE, UTAH



PROFESSIONAL ENGINEER  
REGISTERED  
No. 16234638  
1-11-19  
TIMOTHY JAMES SWISHER  
STATE OF UTAH

NO.	DATE	REVISION	BY
1			
2			
3			
4			
5			
6			
7			
8			

**EROSION/  
CONTROL PLAN**

PROJECT NUMBER  
L2101F

DRAWN BY  
J.MOSS

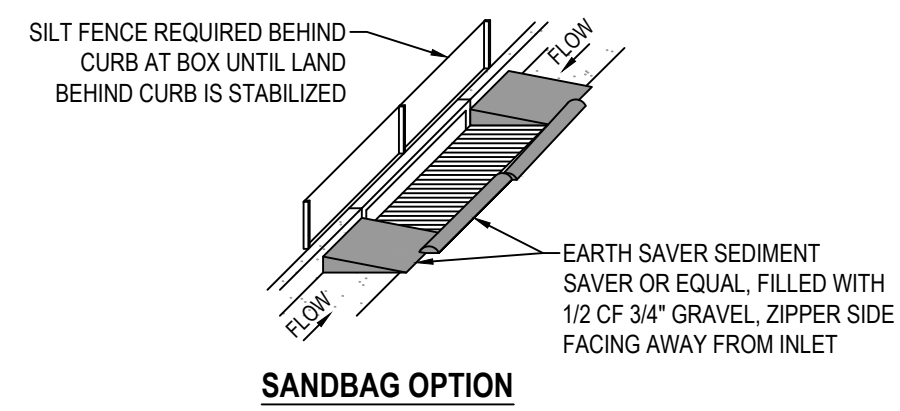
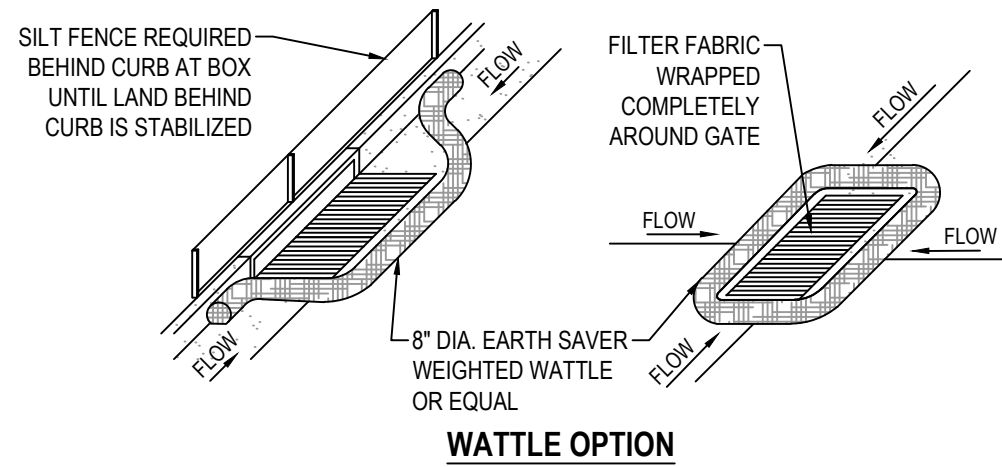
PROJECT MANAGER  
K.RUSSELL

PRINT DATE  
1/11/19

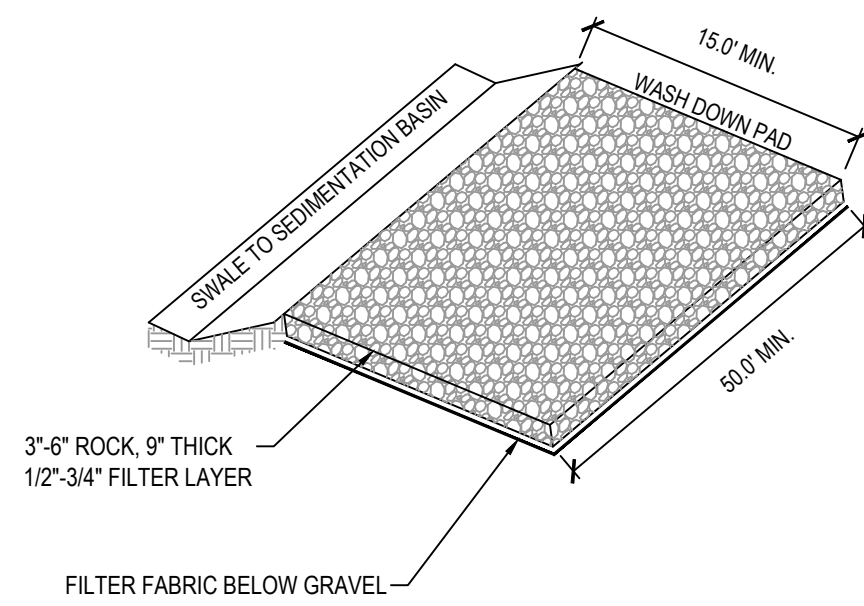
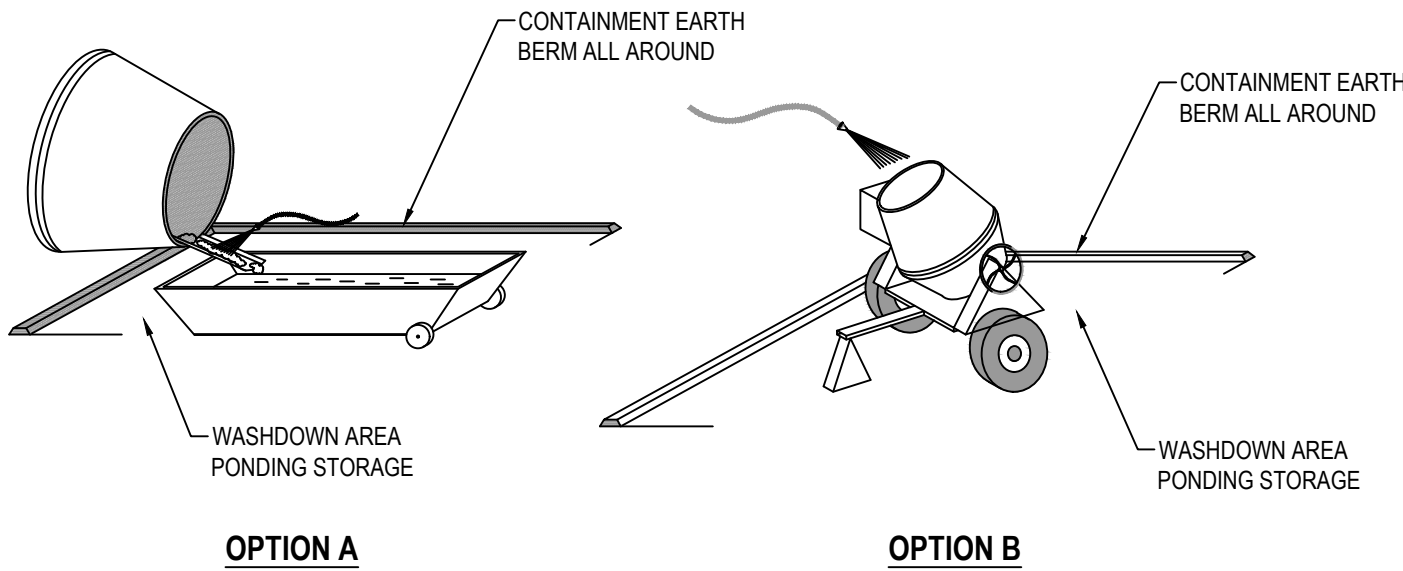
CHECKED BY  
C.PRESTON

**C-400**

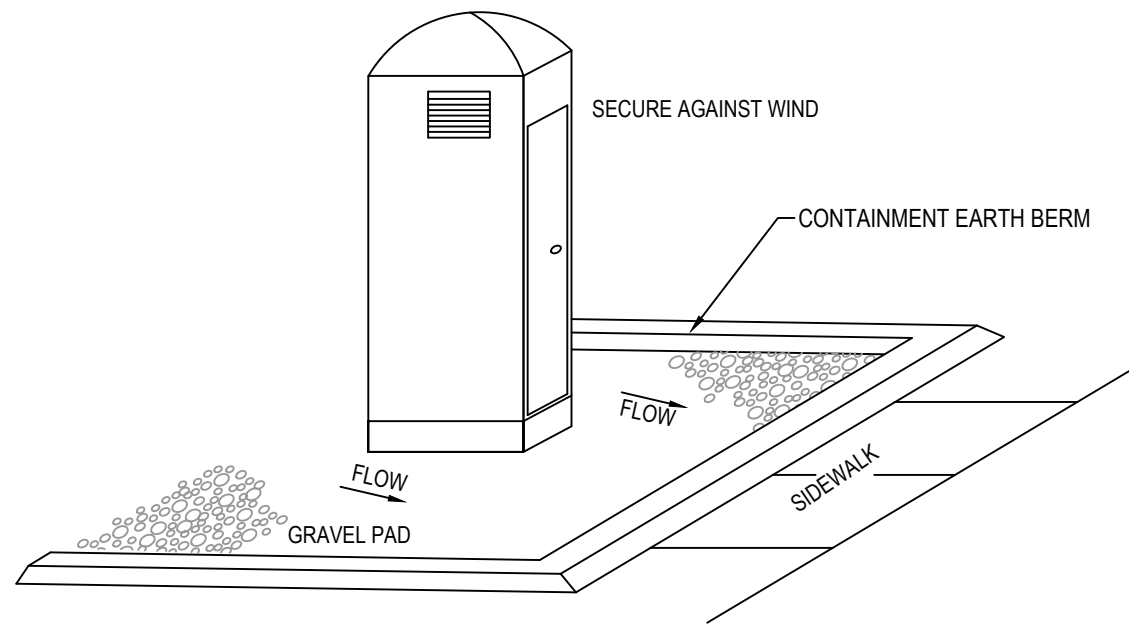




- NOTES:
1. PLACE WATTLES OR GRAVEL BAGS TIGHT AGAINST CURB TO PREVENT SEDIMENT-LADEN WATER FROM SETTING BETWEEN CURB AND WATTLE/BAG.
  2. PLACE WATTLES OR GRAVEL BAGS SUCH THAT FLOW DOES NOT OVERTOP CURB OR ROAD CENTERLINE.
  3. INSPECT INLET PROTECTION AFTER EVERY LARGE STORM EVENT AND AT LEAST 8-WEEKLY, OR PER SWPPP REQUIREMENTS, WHICHEVER IS MORE STRINGENT, TO ENSURE THAT SEDIMENT CONTROL IS MEETING ITS DESIGN INTENT. MAINTAIN AND/OR REPLACE AS NEEDED.
  4. REMOVE SEDIMENT ACCUMULATED WHEN IT REACHES 50% OF GRAVEL BAG OR WATTLE HEIGHT.
  5. CONTRACTOR MAY SUBMIT AN ALTERNATIVE METHOD OF INLET PROTECTION. THE ALTERNATIVE METHOD SHALL BE APPROVED BY THE CITY INSPECTOR AND THE ENGINEER OF RECORD.
  6. BEFORE PLACEMENT OF CURB, STABILIZATION OF LAND BEHIND CURB, AND/OR PAVING, MAINTAIN TOP OF INLET AT 6\"/>



- NOTE:
1. PLACE SIGN ADJACENT TO ENTRANCE \* CONSTRUCTION TRAFFIC ONLY - ALL CONSTRUCTION TRAFFIC SHALL ENTER AND EXIT SITE AT THIS LOCATION\*



## 1 SAG INLET PROTECTION

SCALE: NONE

## 2 CONCRETE WASTE MANAGEMENT

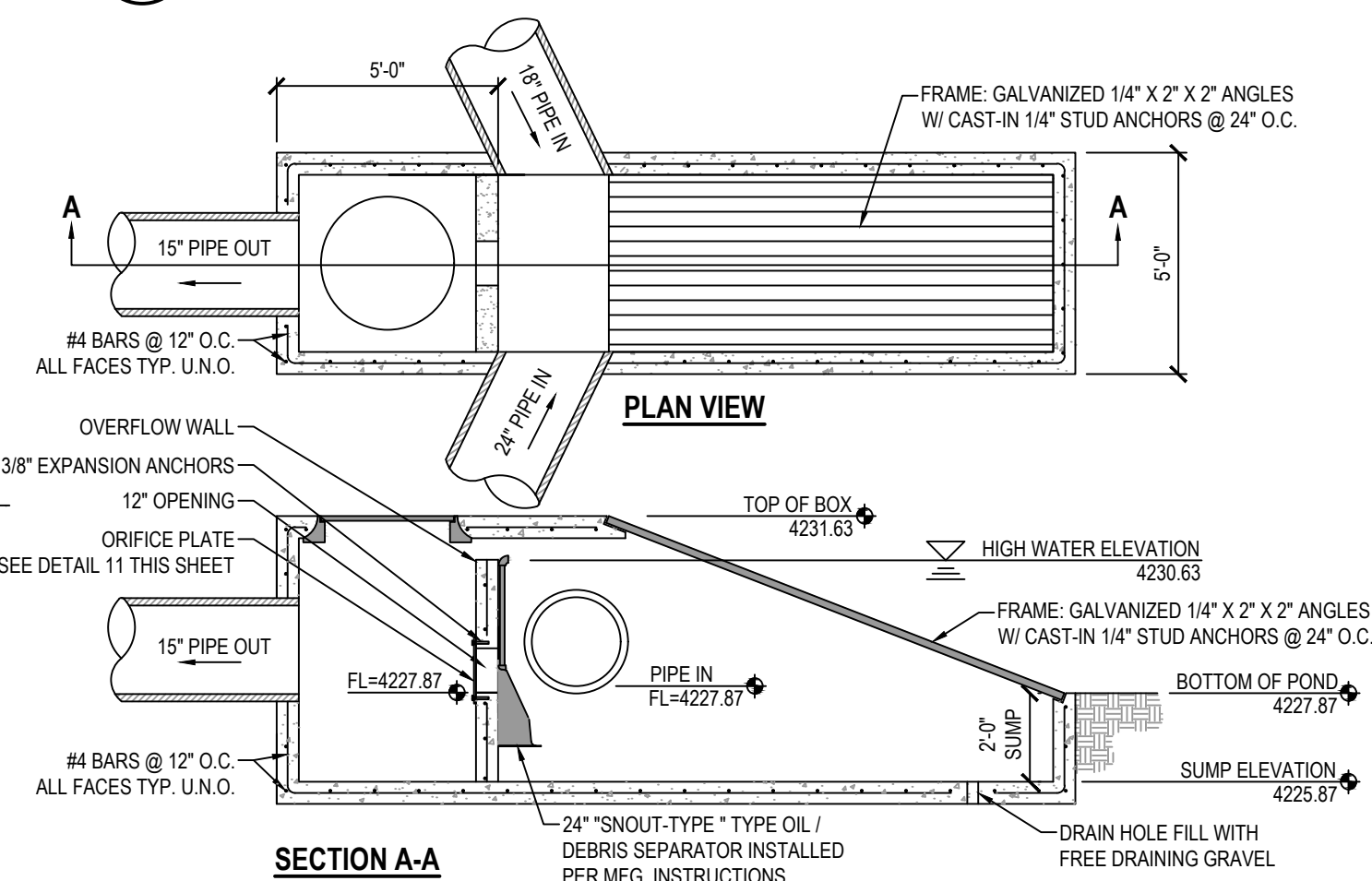
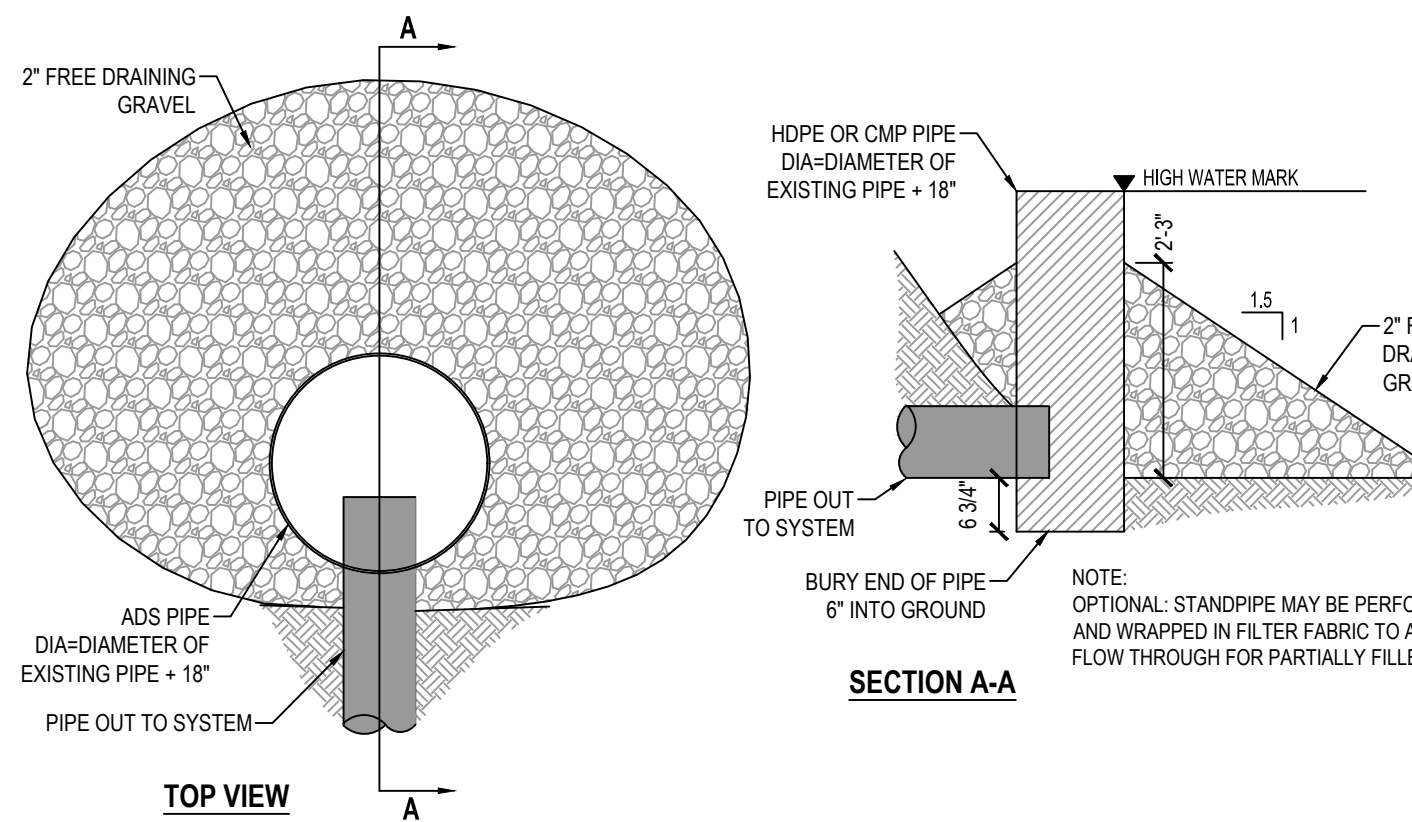
SCALE: NONE

## 3 STABILIZED CONSTRUCTION ENTRANCE

SCALE: NONE

## 4 PORTABLE TOILET

SCALE: NONE

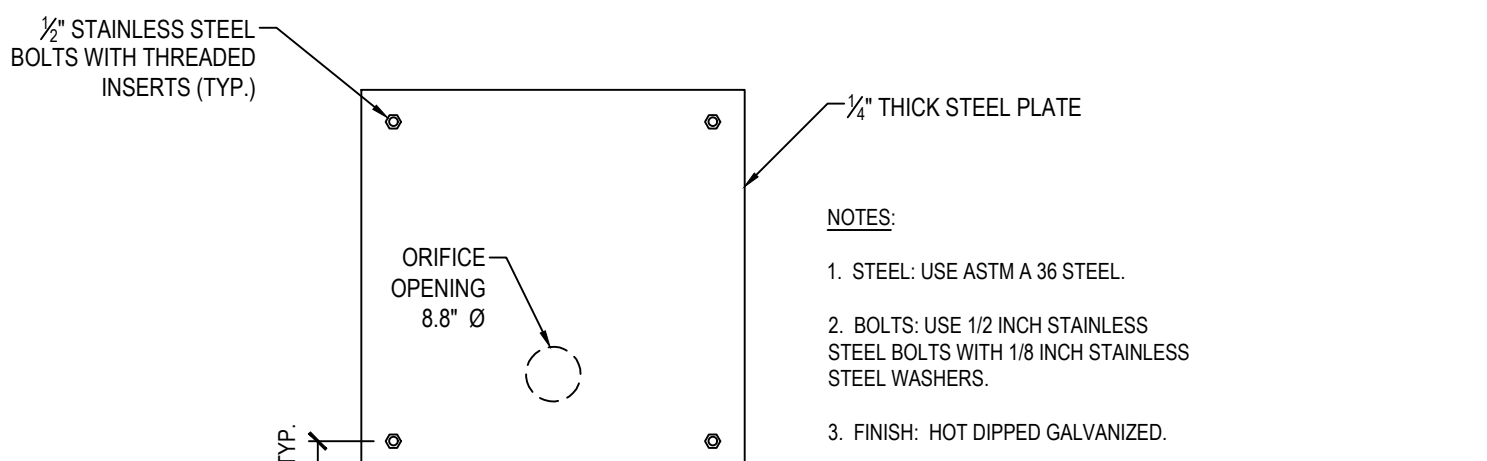
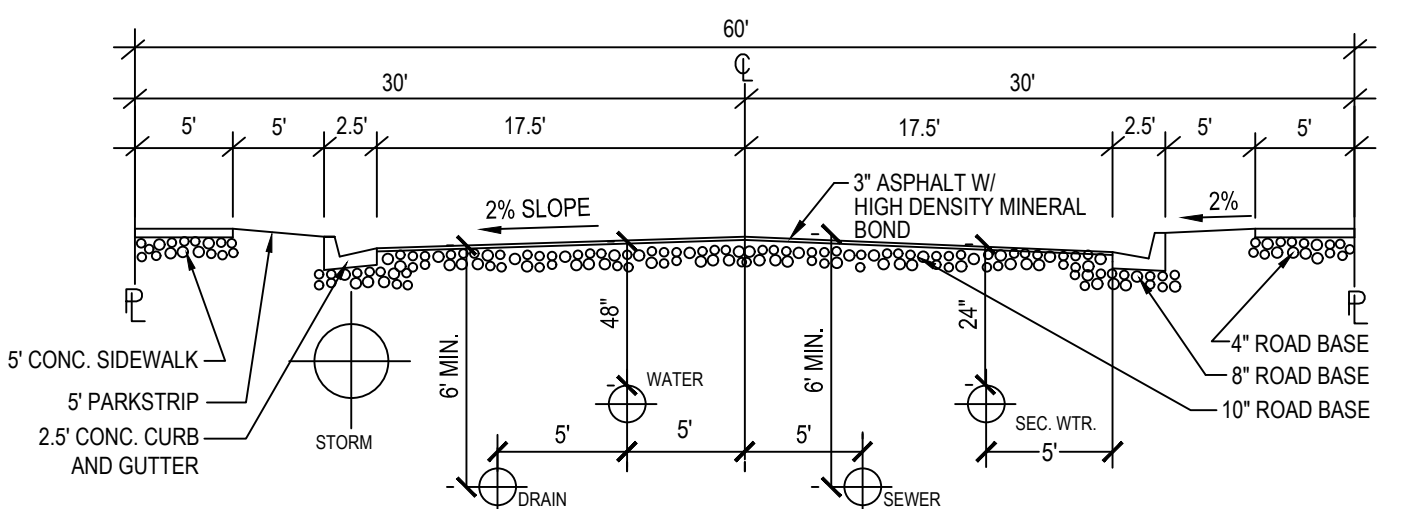


## 8 BASIN CONTROL STRUCTURE #210

SCALE: NONE

## 5 TEMPORARY BASIN OUTLET PROTECTION

SCALE: NONE

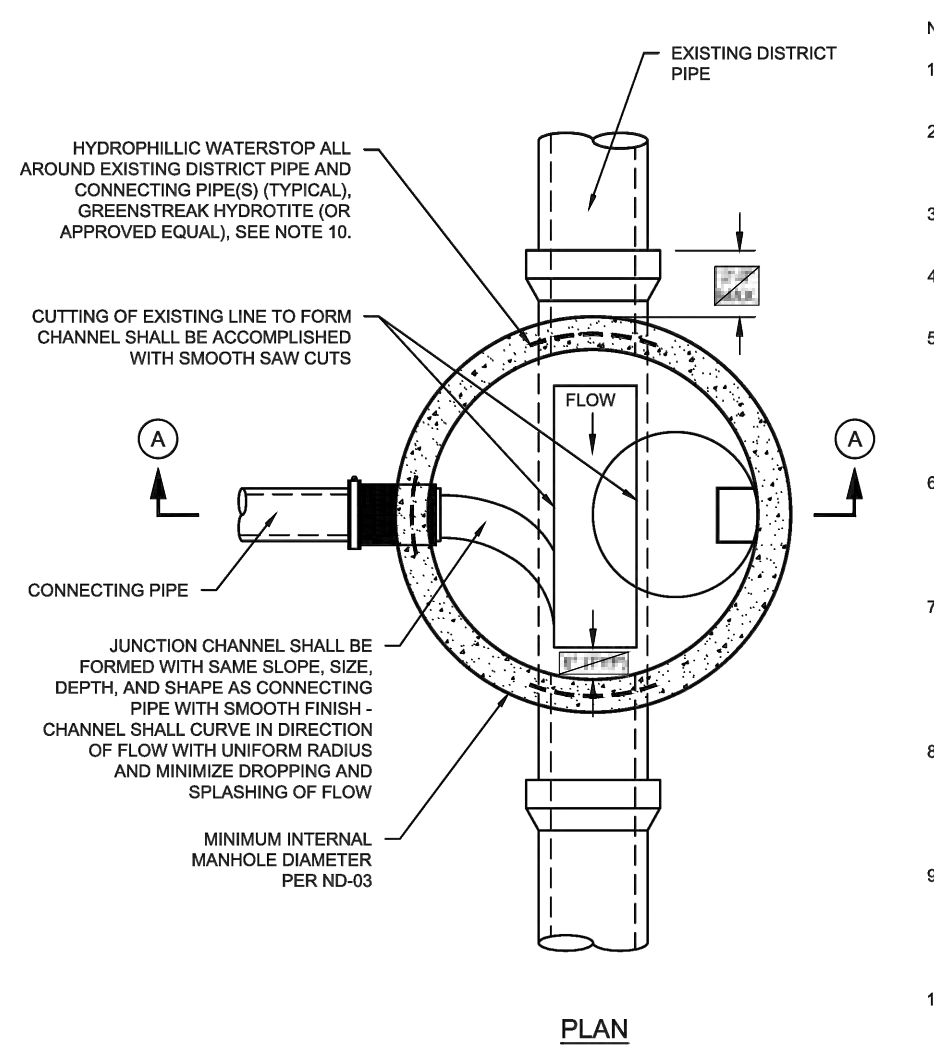


## 11 ORIFICE PLATE

SCALE: NONE

## 10 TYPICAL 60' STREET CROSS SECTION

SCALE: NONE

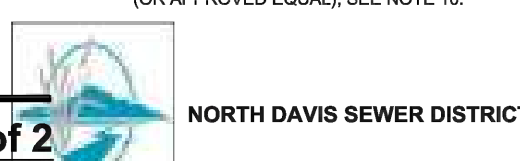


- NOTES:
1. CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER SLOPE AND CONSTRUCTION OF CONNECTING PIPELINE.
  2. CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTION OF EXISTING SEWER AND OTHER UTILITIES, PROPER BACKFILLING, COMPACTING, AND PAVEMENT RESTORATION.
  3. CONTRACTOR SHALL NOTIFY DISTRICT 48 HOURS BEFORE INSTALLATION BEGINS.
  4. CONTRACTOR SHALL GUARANTEE WORK FOR A PERIOD OF ONE YEAR.
  5. PRECAST MANHOLE BASES SHALL BE USED FOR CONSTRUCTION OF ALL NEW DISTRICT SEWER SYSTEMS. PRECAST MANHOLE BASES OR CAST-IN-PLACE MANHOLE BASES MAY BE USED AT THE CONTRACTOR'S OPTION FOR NEW MANHOLES INSTALLED ON EXISTING DISTRICT SANITARY SEWERS. CONTRACTOR SHALL PROVIDE A SUITABLE FOUNDATION IN ALL AREAS.
  6. CAST-IN-PLACE BASE SHALL BE COMPLETED IN A SINGLE POUR. CONCRETE SHALL BE TYPE IV/5 PORTLAND CEMENT PER ASTM C150 WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 4000 PSI PER ASTM C39. NO ADDITIVES SHALL BE PERMITTED WITHOUT PRIOR APPROVAL BY THE DISTRICT.
  7. PRECAST MANHOLE BASES SHALL CONFORM TO ASTM C478. CONCRETE SHALL BE TYPE IV/5 PORTLAND CEMENT PER ASTM C150 WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 4000 PSI PER ASTM C39. CONNECTIONS TO PRECAST BASES SHALL BE FACTORY INSTALLED AND MEET THE REQUIREMENTS OF ASTM C933, 2-10K, PRES-SEAL PSX OR APPROVED EQUAL.
  8. MANHOLES ON C/PPRM OR CIPP LINED DISTRICT SEWERS SHALL BE PRECAST FRP (HOBAS, IF MANUFACTURING ING OR EQUAL) OR POLYMER (ARMOROCK OR EQUAL) UNLESS OTHERWISE APPROVED BY THE DISTRICT. SEE DETAIL ND-12, ND-13 AND ND-14.
  9. GRADE RINGS SHALL BE GROUTED TO PREVENT INFILTRATION. GROUT SHALL BE FULL STRENGTH, NON-METALLIC, NON-SHRINK TYPE V GROUT (MASTERS-LOW T-13, FIVESTAR GROUT OR APPROVED EQUAL) WITH ADDITIVES FOR PROTECTION AGAINST HYDROGEN SULFIDE ATTACK.
  10. PROVIDE HYDROPHILIC WATER STOP BETWEEN PIPE AND CAST CONCRETE, BOTH SIDES, ALL AROUND. CLEAN PIPE USING WIRE BRUSH, POWER WASH OR SAND BLAST TO PROVIDE A CLEAN, DURABLE SURFACE. SECURE WATERSTOP TO PIPE USING MANUFACTURER'S RECOMMENDED METHODS. APPLY LAYER OF GREENSTREAK LEANMASTERS OR APPROVED EQUAL BETWEEN WATERSTOP AND PIPE.



## ND-10 Standard Manhole Connection 1 of 2

Revision 2, Adopted 11/7/2014



## ND-10 Standard Manhole Connection 2 of 2

Revision 2, Adopted 11/7/2014

## 9 NDSO STANDARD MANHOLE CONNECTION

SCALE: NONE

## Series TF-2

- ▶ 100% elastomer construction
- ▶ Will not rust or corrode
- ▶ Will not warp or freeze open or shut
- ▶ Custom-built to customer specifications
- ▶ Low cracking pressure, low headloss
- ▶ Eliminates backflow

Materials of Construction  
Neoprene, Hypalon®, Dura-R, EPDM, Viton®.

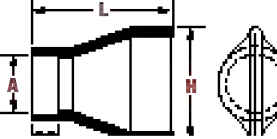
Mounting Bands  
304 or 316 Stainless steel.

The Tidelflex® Check Valve is a revolutionary design for backflow prevention. It offers low cracking pressure to eliminate standing water and very low headloss that is not affected by rust, corrosion or lack of lubrication. Tidelflex® Check Valves are cost-effective because they require no maintenance or repairs and have a long operational life span. Tidelflex® operate using line pressure and backpressure to open and close so no outside energy source is required.

Tidelflex® valves are excellent replacements for ineffective metal flapgate valves because they will not warp or freeze and are virtually maintenance free.

The inside diameter of the TF-2's cuff is constructed to exactly match the outside diameter of the pipe.

The valve is slid onto the pipe and held in place with steel or stainless steel hand clamps, eliminating flanging costs. Tidelflex® TF-2 valves are constructed with a curved bill as standard.



## 12 BACK FLOW PREVENTER - TIDEFLEX TF-2

SCALE: NONE

Pipe O.D.	Length	Bill Height	Cuff Length
INCH	FEET	INCH	FEET
1/2	3	1 1/2	1/2
3/4	3	2	1
1	4	2	1
1 1/2	7	2	1
2	8	4	1
2 1/2	8	4	1 1/2
3	10	4	1 1/2
4	16	9	2
6	16	11	2
8	17	13	2
10	23	17	3
12	31	21	4
14	36	23	4
16	39	27	4
18	39	27	4
20	34	33	5 1/2
22	38	33	6
24	42	39	6
26	42	39	6
28	42	39	6
30	42	39	6
32	42	39	6
34	42	39	6
36	42	39	6
38	42	39	6
40	42	39	6
42	42	39	6
44	42	39	6
46	42	39	6
48	42	39	6
50	42	39	6
52	42	39	6
54	42	39	6
56	42	39	6
58	42	39	6
60	42	39	6
62	42	39	6
64	42	39	6
66	42	39	6
68	42	39	6
70	42	39	6
72	42	39	6
74	42	39	6
76	42	39	6
78	42	39	6
80	42	39	6
82	42	39	6
84	42	39	6
86	42	39	6
88	42	39	6
90	42	39	6
92	42	39	6
94	42	39	6
96	42	39	6



LAYTON  
1485 W. Hill Field Rd., Ste. 204  
Layton, UT 84041  
Phone: 801.547.1100

SALT LAKE CITY  
Phone: 801.255.0529

TOOELE  
Phone: 435.843.3590

CEDAR CITY  
Phone: 435.865.1453

RICHFIELD  
Phone: 435.896.2983

WWW.ENSIGNENG.COM

FOR:  
DR. HORTON, INC.  
12351 S GATEWAY PARK PLACE SUITE D-100  
DRAPER, UTAH 84020  
CONTACT:  
ADAM LOSER  
PHONE: 801-000-0000

CRIDDLE FARMS NORTH  
SUBDIVISION PHASE 2  
900 SOUTH TO 1200 SOUTH 4000 WEST STREET  
SYRACUSE, UTAH



NO.	DATE	REVISION	BY
1			
2			
3			
4			
5			
6			
7			
8			

## DETAILS

PROJECT NUMBER  
L2101F  
DRAWN BY  
J.MOSS  
PROJECT MANAGER  
K.RUSSELL

PRINT DATE  
1/11/19  
CHECKED BY  
C.PRESTON

C-500



## **R-3 ZONING ORDINANCE**

### **10.70.010 Purpose.**

The purpose of this zone is to provide for medium density single-family residential development that conforms to the system of services available.

### **10.70.020 Permitted uses.**

The following, and no others, are uses permitted by right provided the parcel and building meet all other provisions of this title and any other applicable ordinances of Syracuse City.

(A) Accessory uses and buildings (200 square feet or less).

(B) Agriculture.

(C) Churches, synagogues, and temples.

(D) Dwellings, single-family.

(E) Educational services.

(F) Household pets.

(G) Minor home occupations.

(H) Public and quasi-public buildings.

(I) Public parks.

(J) Rabbits and hens.

(K) Residential facilities for persons with disabilities.

(L) Vietnamese potbellied pigs.

### **10.70.030 Conditional uses.**

The following, and no others, may be conditional uses permitted after application and approval as specified in SCC 10.20.080:

(A) Accessory uses and buildings (greater than 200 square feet) (minor).

(B) Apiaries (minor).

(C) Day care centers (major).

(D) Dwellings, accessory (major/minor, see SCC 10.30.020).

(E) Home occupations (major).

(F) Temporary commercial uses (see SCC 10.35.050) (minor).

(G) Temporary use of buildings (see SCC 10.30.100(A)(12)) (minor).

**10.70.040 Minimum lot standards.**

All lots shall be developed and all structures and uses shall be placed on lots in accordance with the following standards:

(A) Density: minimum lot size 8,000 square feet, but in no case shall the density exceed 4.0 lots per gross acre.

(B) Lot width: 80 feet.

(C) Front yard: 25 feet.

(D) Side yards: Eight feet both sides.

(E) Rear yard: 20 feet.

(F) Building height: as allowed by current adopted building code.

(G) Variation of lot: the Land Use Authority may reduce the lot width requirement in particular cases when a property owner provides evidence they acquired the land in good faith and, by reason of size, shape, or other special condition(s) of the specific property, application of the lot width requirement would effectively prohibit or unreasonably restrict the ability to subdivide the property or a reduction of the lot width requirement would alleviate a clearly demonstrable hardship as distinguished from a special privilege sought by the applicant. The Land Use Authority shall approve no lot width reduction without a determination that:

(1) The strict application of the lot width requirement would result in substantial hardship;

(2) Adjacent properties do not share generally such a hardship and the property in question has unusual circumstances or conditions where literal enforcement of the requirements of the zone would result in severe hardship;

(3) The granting of such reduction would not be of substantial detriment to adjacent property or influence negatively upon the intent of the zone;

(4) The condition or situation of the property concerned or the intended use of the property is not of so general or recurring a nature as to detract from the intention or appearance of the zone as identified in the City's general plan.

**10.70.050 Off-street parking and loading.**

Off-street parking and loading shall be provided as specified in Chapter 10.40 SCC.

**10.70.060 Signs.**

The signs permitted in this zone shall be those allowed in residential zones by Chapter 10.45 SCC.



## **Criddle Farms North Subdivision Phase 2**

975 South Street & 4100 West Street

Engineer Final Plan Review

*Completed by Brian Bloemen on January 2, 2019*

1. End the culinary main with an air vac after the service for Lot 222 (It does not need to extend to the phase line).
2. End the west end of the secondary main in 920 South Street with a drain into Combo Box #207. Adjust the mainline alignment so it is not over the sewer main.
3. Remove the note to install an Eclipse 88 sampling station on PP-5.
4. PP-6 and PP-7 show different elevations for land drain MH#328.
5. Install a tide flex on the land drain outfall.
6. The storm drain outfall on the west side of 4500 West appears to be going through private property. Verify the ownership and if it is going through private property an easement will be necessary.
7. Change Layton Parkway to 4000 West Street.

If you have any further comments or questions, please feel free to contact me at 801-614-9630.

Sincerely,

Brian Bloemen, P.E.  
City Engineer



TO: Community Development, Attention: Noah Steele  
FROM: Jo Hamblin, Fire Marshal  
RE: Criddle Farms North phase 2

DATE: January 3, 2019

I have reviewed the plans for the above referenced project. The Fire Prevention Division of this department has the following comments/concerns.

1. 870 south street is over 150 feet and will require a temporary turnaround.

These plans have been reviewed for Fire Department requirements only. Other departments must review these plans and will have their requirements. This review by the Fire Department must not be construed as final approval from Syracuse City.



## Final Subdivision Plan Review

**Mayor**

Mike Gailey

**City Council**

Corinne Bolduc

Lisa Bingham

David Maughan

Jordan Savage

Doug Peterson

**City Manager**

Brody Bovero

1/2/2019

Dear Applicant,

The Syracuse City Community and Economic Development Department has conducted a review of the Criddle Farms North Phase 2 Final Plan for compliance with the city's adopted land use ordinance. Please refer to the following table for items that require additional attention.

1. Add a note indicating ownership and maintenance for parcels A and B.
2. Note 3 should say "Street trees..."
3. All truncated dome plates should align in accordance with ADA requirements.
4. Sewer easement specifics on the neighboring property should be finalized before the plat is recorded with Davis County.

Please contact me with any questions concerning this project.

Regards,

Royce Davies

City Planner

(801) 614-9632

[rdavies@syracuseut.com](mailto:rdavies@syracuseut.com)



3200 West 300 North  
West Point, Utah, 84015  
(801) 776-0971  
Fax (801) 525-9150  
www.westpointcity.org

Enhancing Quality of Life

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## MEMORANDUM

To: Syracuse Planning Dept.

From: Boyd Davis, P.E. *B.D.*

cc: Ensign Engineering

**RE: Criddle Farms Utilities & Easement Review**

Date Plans Received: Dec. 20, 2018

Date Reviewed: January 8, 2019

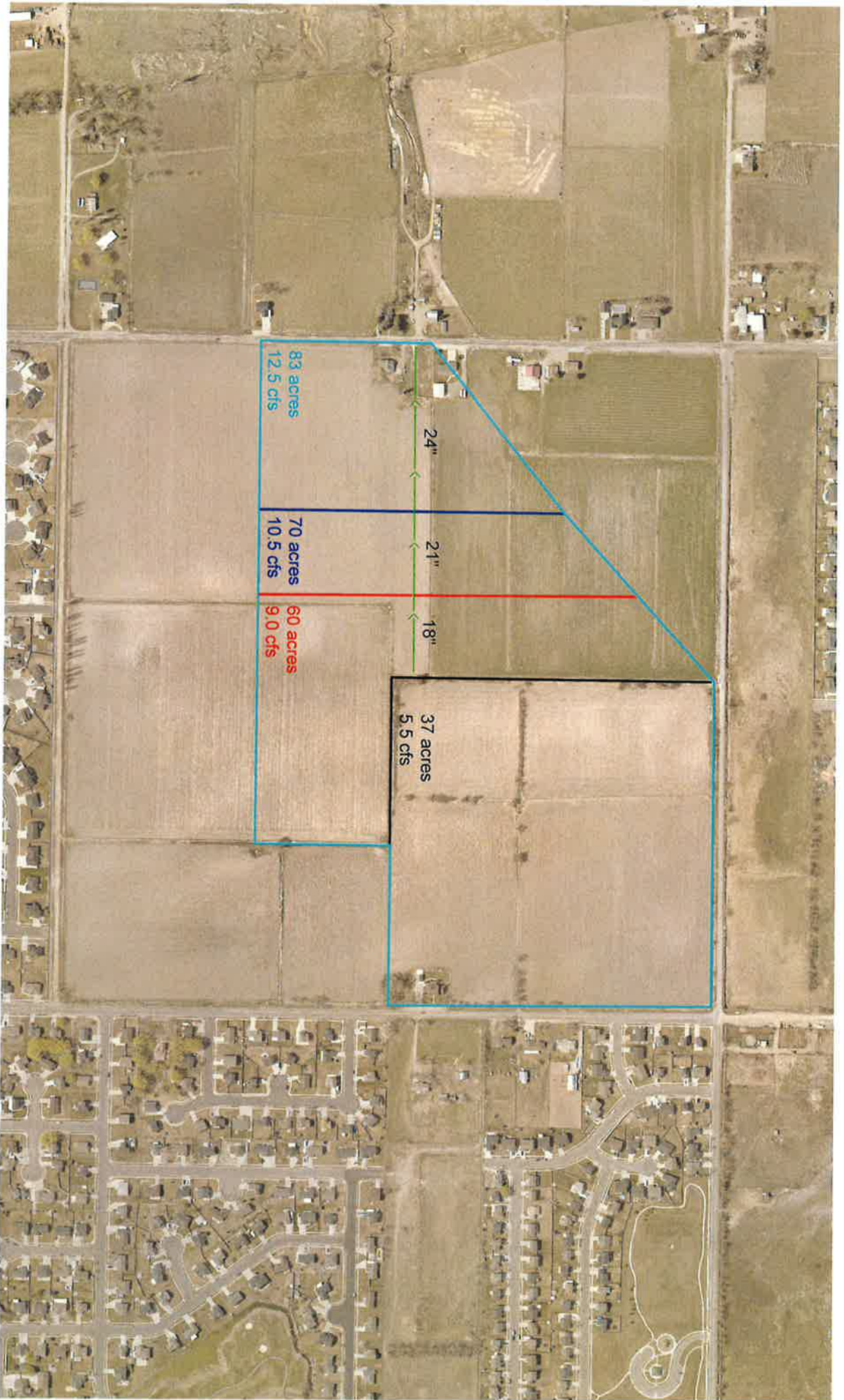
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I have completed a review of the final plan for the subject subdivision and offer the following comments for your review:

1. Storm Drain
  - a. Please submit documentation of approval by Davis County Public Works.
  - b. Please submit documentation of approval by UDOT to cross SR 110.
  - c. I recommend that the pipe size be modified as shown on the attached sketch. I understand that there will be a meeting with Davis County to discuss the drainage. My comments are subject to change based upon that meeting.
2. Sewer
  - a. Please submit documentation of approval by the North Davis Sewer District.
3. Land Drain
  - a. No comments.
4. Easement
  - a. Please submit a copy of the easement document for our review.
  - b. The note on sheet PP-9 should state that the easement is in favor of both Syracuse & West Point.
  - c. There is no temporary easement east of station 122+00. I would expect that there is a need for the temporary construction easement along the entire length.
  - d. Originally, we were told that the permanent easement would be 30', but it is shown as 40'. Please verify that this is correct.
  - e. The plat shows a legal description for a boundary line agreement. Has an agreement been signed? If so, please submit a copy of the agreement.

**Recommendation:** I would recommend that these items be addressed before construction begins. West Point City would like to be invited the pre-construction meeting.







# PLANNING COMMISSION AGENDA

January 15, 2019

**Agenda Item # 6**

**2019 Thought/Pledge Schedule**



## PC Schedule 2019

Meeting Date		Invocation/Thought	Pledge
January	1	<i>cancelled (holiday)</i>	
January	15	Day	Thorson
February	5	Vaughan	Rackham
February	19	McCuistion	Johnson
March	5	Thorson	Day
March	19	Rackham	Vaughan
April	2	Johnson	McCuistion
April	16	Day	Thorson
May	7	Vaughan	Rackham
May	21	McCuistion	Johnson
June	4	Thorson	Day
June	18	Rackham	Vaughan
July	2	Johnson	McCuistion
July	16	Day	Thorson
August	6	Vaughan	Rackham
August	20	McCuistion	Johnson
September	3	Thorson	Day
September	17	Rackham	Vaughan
October	1	Johnson	McCuistion
October	15	Day	Thorson
November	5	Vaughan	Rackham
November	19	McCuistion	Johnson
December	3	Thorson	Day
December	17	Rackham	Vaughan

*\* subject to change without notice*



# PLANNING COMMISSION WORK SESSION

January 15, 2019

## Agenda Item # 2a

## IBI Antelope Drive Land Use Study Report

### *Factual Summation*

Please review the following information. Any questions regarding this agenda item may be directed to Noah Steele, Community & Economic Development Director.

The city recieved a grant from the Wasatch Front Regional Council (W.F.R.C.). The grant is called Transportation and Land Use Connection (T.L.C.)

"The TLC program provides technical assistance to local communities to help them achieve their goals and plan for growth. The program helps communities implement changes to the built environment that reduce traffic on roads and enable more people to easily walk, bike, and use transit. This approach is consistent with the Wasatch Choice Vision and helps residents living throughout the region enjoy a high quality of life through enhanced mobility, better air quality, and improved economic opportunities."

We chose to hire IBI to perform the study. The firm is a very qualified firm with a team of talented urban designers, landscape architects, planners, and mapping professionals. They also consulted with Zion's bank to provide a market study and feasibility study.

The planned West Davis Corridor will effect our city in many ways. It will surely bring a high demand for development within our city, increase traffic, and alter commuting patterns. Using this study will help us ensure that our planned land uses are feasible, realistic, and will help us capture the highest and best use of the land along this major thoroughfare.

Please find attached the draft study. IBI will be presenting the final version during the meeting.





# Syracuse Town Center Vision Plan + Design Guidelines

Draft October 2018









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# INTRODUCTION









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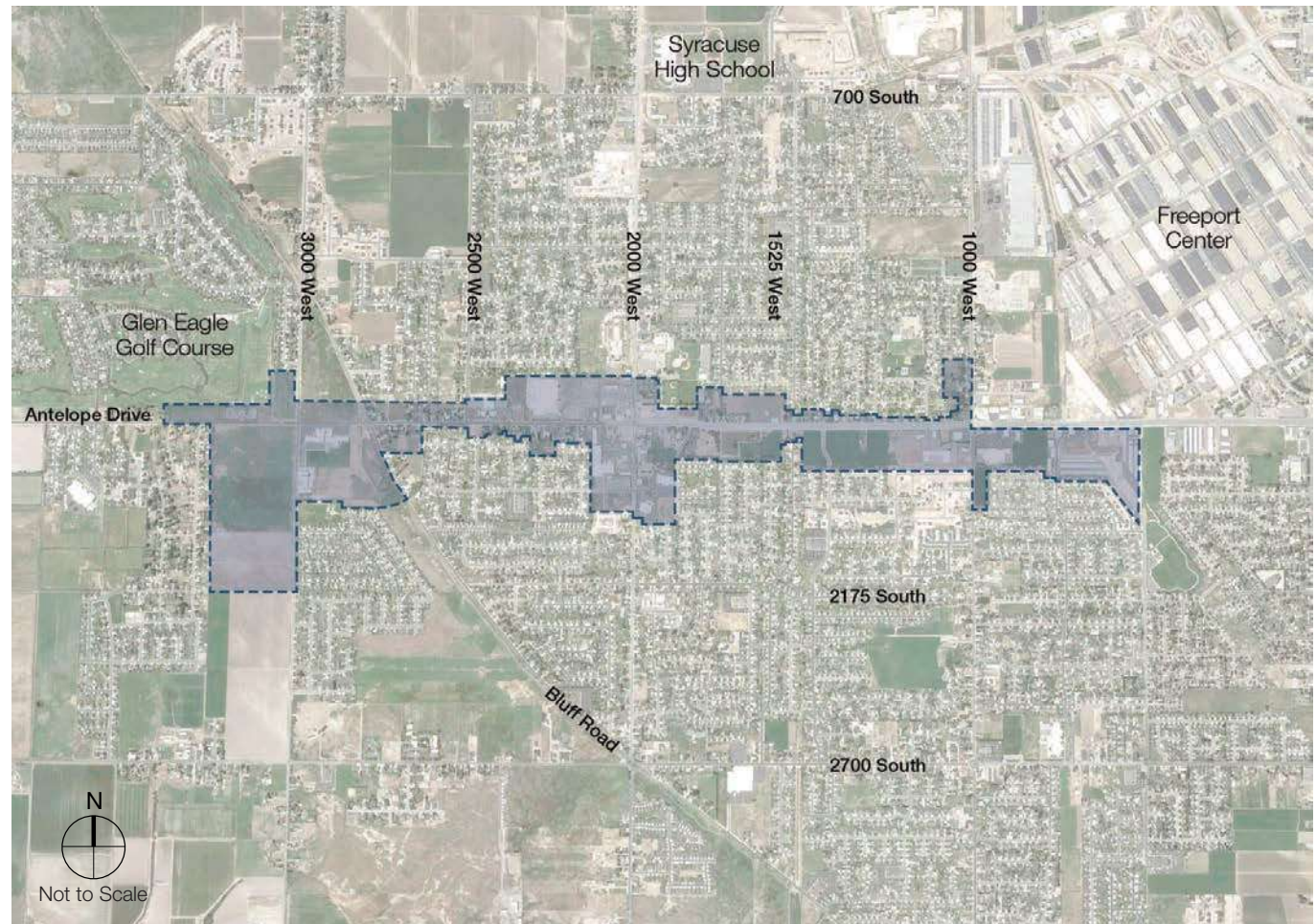
## Statement of Purpose

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This document provides the vision and design guidelines that will guide the development within the study area along the Antelope Drive Corridor. It is accompanied by a concept development plan and establishes urban design best practices and quality standards for development.

These standards and guidelines will provide clear direction to developers and end-users within the study area. Consistent application of these standards creates a cohesive development through the thoughtful arrangement and orientation of buildings, streets, and open space. These guidelines also ensure that projects meet quality standards and that development will have a consistent “look and feel.”

Using Davis County and the Northern Utah Region as a contextual guide, these guidelines will offer direction on an over-arching architectural style.



Study Area Boundary

## Project Location & Context

Syracuse City is located north of Salt Lake City and sits between the Wasatch Mountains and Great Salt Lake. Known as the “Gateway to Antelope Island,” Syracuse City is located in a beautiful natural setting with plentiful recreational opportunities and welcoming residents.

### TRAILS

Syracuse City has shared use trails, bike lanes, shared lanes, and equestrian trails that connect several parks within the City and provide connectivity between the Bird Refuge, Antelope Island and West Point City to the north.

The Syracuse Emigrant Trail is a historic trail that began as a connecting road between City of Rocks, Idaho and Salt Lake City. The segment that runs through Syracuse City is now a popular multi-use path that runs adjacent to Bluff Road. The Great Salt Lake Shoreline Trail and Trail Access points are also integral to the city’s trail system.

### GOLF COURSE

The Glen Eagle Golf Course is patterned after those located on the shores of Scotland. Scenic rolling hills feature a touch of wetlands that offer a challenging golfing experience with breathtaking

views of the Wasatch Mountains. This golf course acts as a regional draw and an amenity to the residents of Syracuse City.

### OUTDOOR RECREATION AND TOURISM

Antelope Island State Park sits a mere 12 miles from Syracuse and offers visitors many recreational opportunities, including beaches, grills, hiking and horseback riding trails, and bike facilities. The State Park also provides a Visitor’s Center, art displays, exhibits, gift shop, and the historic Fielding Garr Ranch. Given the City’s close proximity to Antelope Island, Syracuse has an opportunity to capitalize on this tourism, inviting visitors to not just pass through the city but find reason to stay.

### VEHICULAR ACCESS

Syracuse City is easily accessed from Interstate 15 (I-15) off of Utah State Road 108-N/ W 2000 N/ Antelope Dr. Syracuse has an established network of local roads providing access through central points, connecting to different amenities and access points throughout most of the community.

UDOT manages Antelope Dr. and will influence ingress/egress, traffic speed, parking, and general traffic flow. The street widths, parking options, street vegetation and bike facilities will all be addressed in this document as there are

opportunities for improvement.

### LAND USES

Currently, the development in Syracuse City consists of several established residential communities, 11 city parks, an extensive trail system, and an 18 hole golf course. The majority of commercial uses in the City are located in Antelope Drive Corridor.



Antelope Island State Park Causeway



Bluff Trail



Hot air balloons







## Place Character + Materiality

Syracuse City is located in a unique and celebrated geographical region, adjacent to the Great Salt Lake, Antelope Island State Park and Wasatch Mountains. The area has a rich cultural heritage, tying back to Native Americans and, more recently, Mormon Pioneers.

In the present day, the area is faced with responsibly accommodating a growing population of residents and visitors. Part of that challenge is to seamlessly integrate a re-imagined urban downtown with a mixed-use center of commerce, employment opportunities, residential development and multi-modal connections.

The City also has the unique position of sitting adjacent the West Davis Corridor, a further connection to the city by visitors.

Inspiration for place character and materiality can be drawn from many locally appropriate sources. It is important that any inspiration is then adapted to fit these design guidelines and, thus, the vision for the Syracuse City Community.















# VISION + GUIDING PRINCIPLES



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THE PURPOSE OF THIS VISION PLAN & DESIGN GUIDELINES IS TO DIRECT **THE EVOLUTION OF THE SYRACUSE TOWN CENTER CORRIDOR BY PROMOTING QUALITY DEVELOPMENT AND THOUGHTFUL DESIGN.** THIS WILL BE ACCOMPLISHED THROUGH THE CREATION OF UNIQUE ACTIVITY NODES THAT INCREASE INTEREST AND INVESTMENT IN SYRACUSE CITY BY DEVELOPING A COMMUNITY CORE THAT SUPPORTS ECONOMIC RESILIENCE, EMPLOYMENT OPPORTUNITIES, SAFE TRANSPORTATION, COMMERCIAL CHOICES, RECREATION OFFERINGS. AND CREATES AN AUTHENTIC AND VIBRANT HEART FOR SYRACUSE CITY AND THE NORTH DAVIS REGION.



## Overarching Goals

### THE FUTURE OF SYRACUSE

Syracuse is currently one of the fastest growing cities in Utah, and is on the precipice of significant change that will have a major influence on the the future of the city for decades to come. The driving force of this change will be brought by the construction of the West Davis Corridor and the widening of 2000 West and Antelope Drive, which will lead to even faster growth, altered transportation patterns, and easier access into the City for visitors. Additionally, the growing number of visitors to Antelope Island State Park also presents an exciting opportunity for the City to capitalize on.

The vast majority of Syracuse's commercially zoned parcels fall in the study area of this plan. The long term success and viability of this corridor will be paramount to the City's economic resiliency and represents the City's best location to create a great place for both residents and visitors.

Three overarching goals have been established to both address the challenges and capitalize on the opportunities that face Syracuse.

### INCREASE DAYTIME POPULATION

Increase daytime population and opportunity to live and work within Syracuse, thus becoming a regional commercial destination.

A vibrant workforce strengthens the tax base through diversification and worker spending. In order to achieve this, office and commercial uses are to be integrated into mixed-use areas in addition to pursuing the development of a business park and more inviting places to eat, shop and recreate within the City.

### CAPTURE TOURISM - BECOME THE DESTINATION

Syracuse City is "the Gateway to Antelope Island State Park" and is poised to benefit from its continuing increase in visitation.

The business districts and activity nodes developed along the Antelope Drive Corridor will provide opportunities and incentives for visitor's to Antelope Island to stay and spend money in Syracuse. These nodes will include restaurants, hotels, and other commercial amenities, such as outdoor recreation equipment retailers.

The proximity of Antelope Island also provides an exciting branding opportunity that will distinguish Syracuse from surrounding cities.

### IMPROVE MOBILITY AND SAFETY

Street, sidewalk and trail systems will provide safe, convenient and clear access between business districts, residential areas, commercial centers and recreation facilities. These networks will also promote safety as a paramount objective, with emphasis on walkability that will also support healthy lifestyles and wellbeing by creating a safe and comfortable environment that separates pedestrians from vehicular circulation.

## Guiding Principles

The following guiding principles apply generally to all nodes within the study area. These guiding principles are used to inform and steer the decisions of this Vision Plan and Design Guidelines.

### CREATE UNIQUE NODES OF INTEREST

Development nodes will be created in key areas along Antelope Drive, and will build upon on the exiting Town Center Node at 2000 West.

Each node created will become a unique district with a distinguished character that presents a clear vision for land-uses and the intended user experience. This will establish a clear and organized framework for development in the Corridor that will act as a catalysts for private investment.

### CREATE MIXED-USE NEIGHBORHOODS

Each development node will feature a strategic mix of land-uses that strengthens the vision for each node by creating the appropriate mix of office, retail, residential, hospitality and other uses.

### CREATE “PLACES”

Each node will be developed to become a great “place” through quality architecture, landscape architecture, and urban design. They will have uses that draw users, such as restaurants and shops, as well as public amenities such as outdoor dining, plazas, parks, and walkable streets.





### MINIMIZE IMPACTS TO EXISTING NEIGHBORHOODS

Existing neighborhoods in Syracuse will be preserved by channeling the projected future growth into development nodes along Antelope Drive.

The greatest intensity of development will be located near major roads, such as Antelope Drive and the future West Davis Corridor. New development near existing neighborhoods will step down in intensity and height.



### DEVELOP A STRONG IDENTITY

The Syracuse Town Center and will adhere to quality development standards and guidelines that will distinguish the City as a great place to live, work and visit. It will also establish a unique and identifiable aesthetic and brand that gives the entire Antelope Drive Corridor a cohesive look and feel.



### ENCOURAGE HIGH QUALITY DEVELOPMENT

Development in Syracuse City of business districts, commercial districts and multi-family housing will be held to a high standard and integrate good design principles, thoughtful architectural design and be constructed with high-quality, long lasting materials.



### PROMOTE WALKABILITY AND BIKABILITY

Commercial areas will be designed for safe cycling and walking through the arrangement of buildings on a site, designing buildings to face streets, and creating façades that have human-scale features. Other safety measures include traffic calming, protected bike lanes, pedestrian crossing signals, and clear signage.









# FRAMEWORK PLANS

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## Plan Framework

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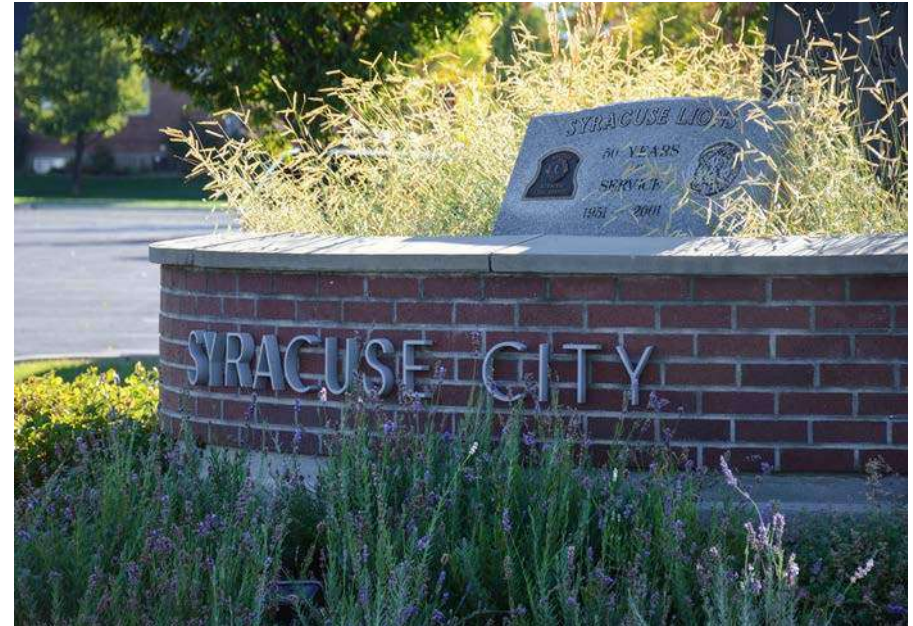
### OVERVIEW + OBJECTIVE

Currently, the majority of commercial uses in Syracuse surround the Antelope Drive Corridor, with the Town Center at 2000 West, as well as another commercial node near at the 1000 West intersection.

The framework for this plan continues the concept of creating commercial nodes along Antelope Drive by strengthening existing nodes and creating a new node near the future West Davis Corridor.

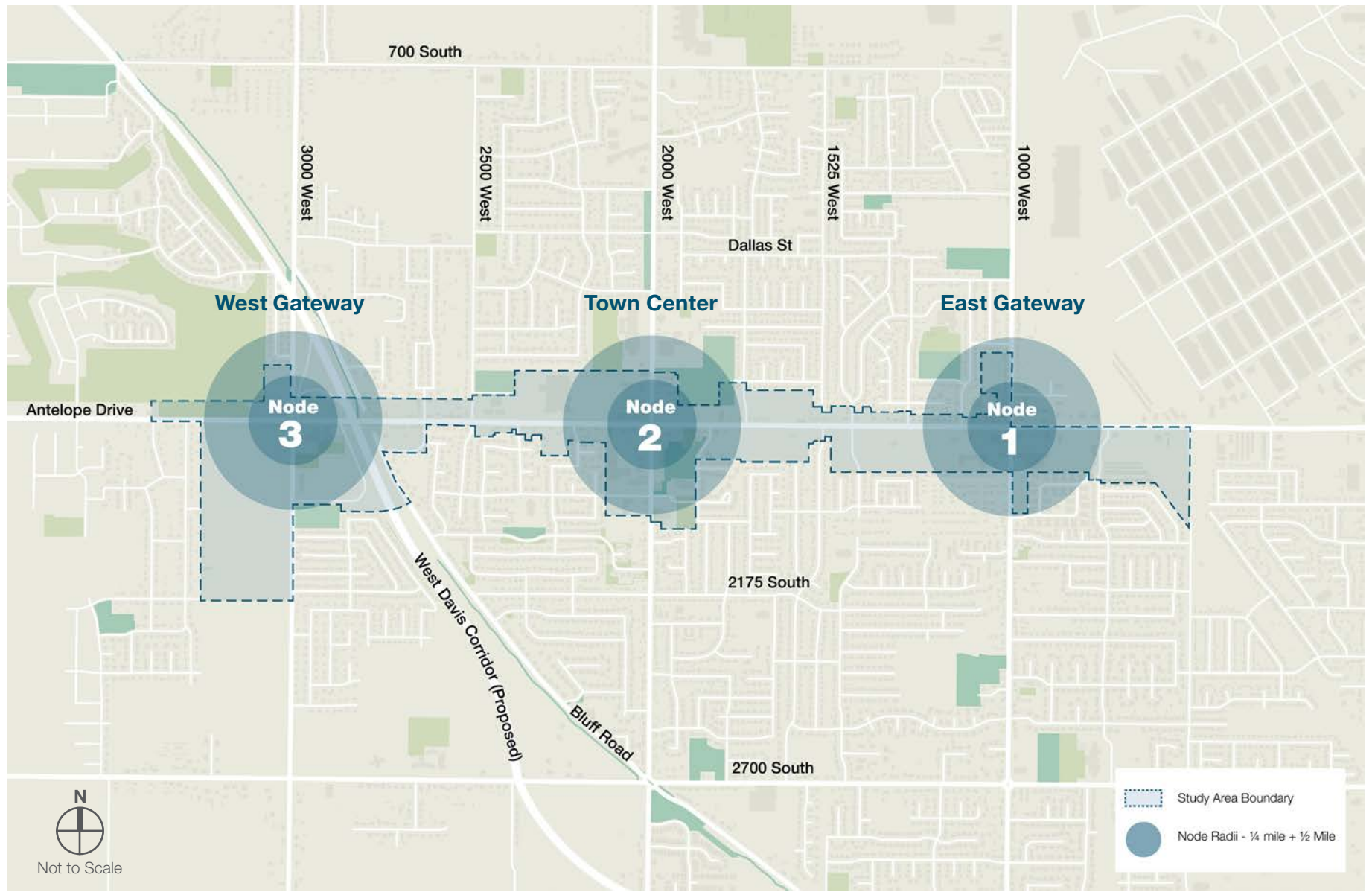
Focusing development intensity and a mix of land-uses within these nodes will create the critical mass of development needed to create a “place” that is viable, vibrant, and active. Each node will respond to economic realities by targeting specific and strategic land-use and transportation options, establishing a unique yet complimentary character for each.

This organization in turn allows for the areas between nodes to maintain supportive land uses and creates a sequence of commercial and residential uses, rather than one long run of commercial uses that unnecessarily compete with each other.



Small Plaza with Outdoor Dining in the Town Center Node





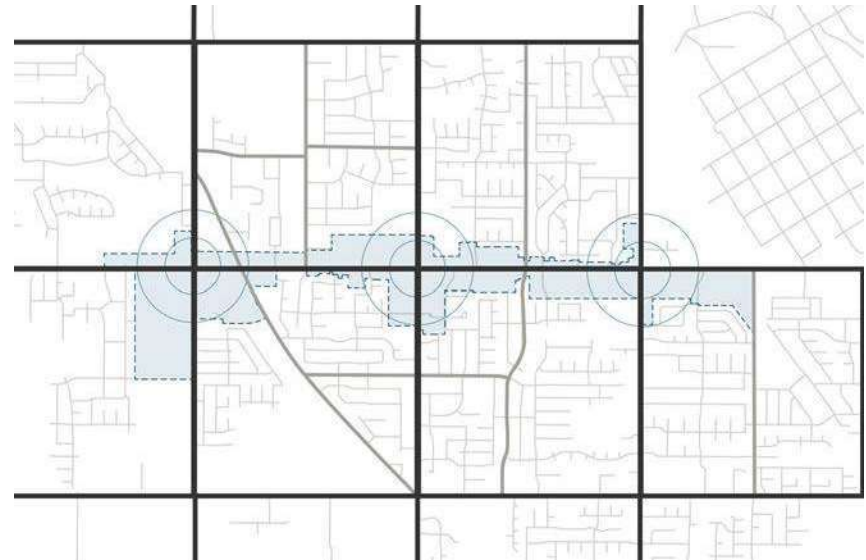
## Transportation + Transit

### OVERVIEW + OBJECTIVE

Like many suburban Utah communities, the structure of Syracuse's road network begins with a large grid of wide streets, which are spaced at approximately 1 mile apart. Within this large grid is a network of collector and local streets, which generally have limited connectivity, when compared to a more walkable community with a traditional grid. This system is designed for vehicle traffic, and often creates safety, convenience, and comfort issues for pedestrians, bicycles, and other transportation modes. As growth in the City continues, it's essential that measures are taken to improve safety and convenience for these users.

One of the biggest challenges is the ability to safely cross the major streets in the City, especially Antelope Drive and 2000 West.

The construction of the West Davis Corridor will have significant influence on future traffic patterns. Historically, the traffic flow along Antelope Drive has mostly gone from East to West. However, with the creation of the West Davis Corridor, this traffic pattern is anticipated to flip and improve the current level of service.

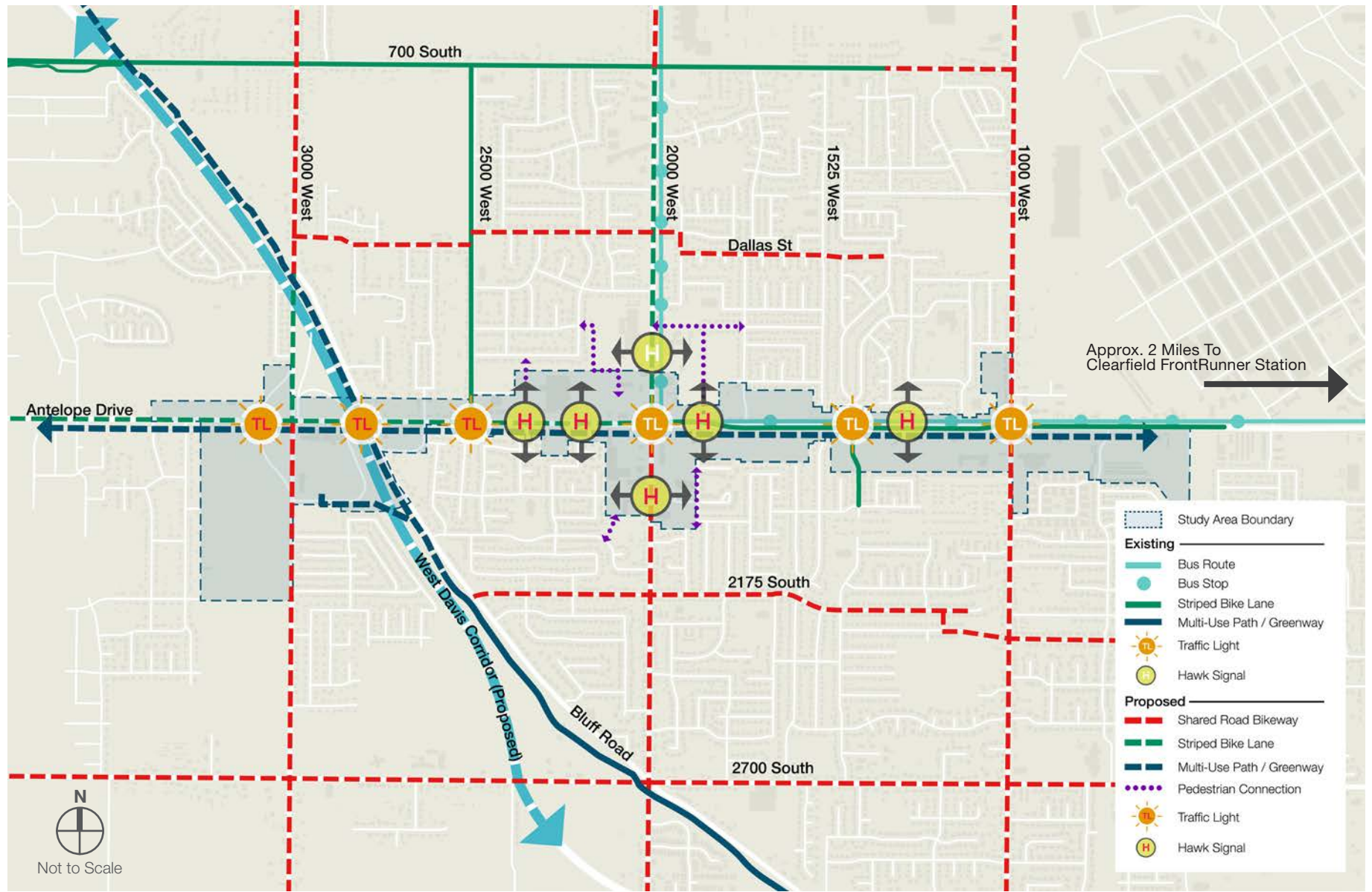


Syracuse Road Network - Levels of Street Types



Antelope Drive at 2000 West





## Greenway + Open Space Network

### OVERVIEW + OBJECTIVE

Open space is a vital part of a healthy community, and as the development intensifies in the identified nodes, open space and recreation amenities become even more essential. Fortunately, there is currently a number of parks within, or adjacent to the nodes.

The existing parks in the study area are similar in nature, and provide similar experiences. The amenities provided include open grass fields, children's playgrounds, and various sports fields/courts. A splash pad/water playground is also currently being constructed in the Town Center Node.

As future open spaces are developed, care should be taken to provide other types of open spaces and experiences that are not currently available. A few examples include plazas, greenways, ecological parks, heritage farm and dog park. Public open space should be provided at different scales and provide different experiences for people of all ages and abilities. The size and type of open space constructed should generally respond to the land-uses in the area to provide appropriate amenities for likely users.

This plan encourages the development of a major greenway trail along Antelope Drive. This trail will connect all of the nodes together and act as a central spine that encourages bicycle, pedestrian and family use to connect to the existing and proposed open spaces.

New open spaces proposed in the study area include:

- › 2000 West Linear Park
- › West Gateway Farmer's Heritage Park
- › Smaller parks, plazas and open spaces in each node

Generally, each node has or should pursue the implementation of 2-3 open spaces such as smaller parks and plazas.

The Town Center node will act as the heart of the community as it contains the City's celebratory open spaces. Additionally, these spaces should be considered accessible throughout the week and not just on weekends and during annual celebrations.



Founder's Park



Centennial Park

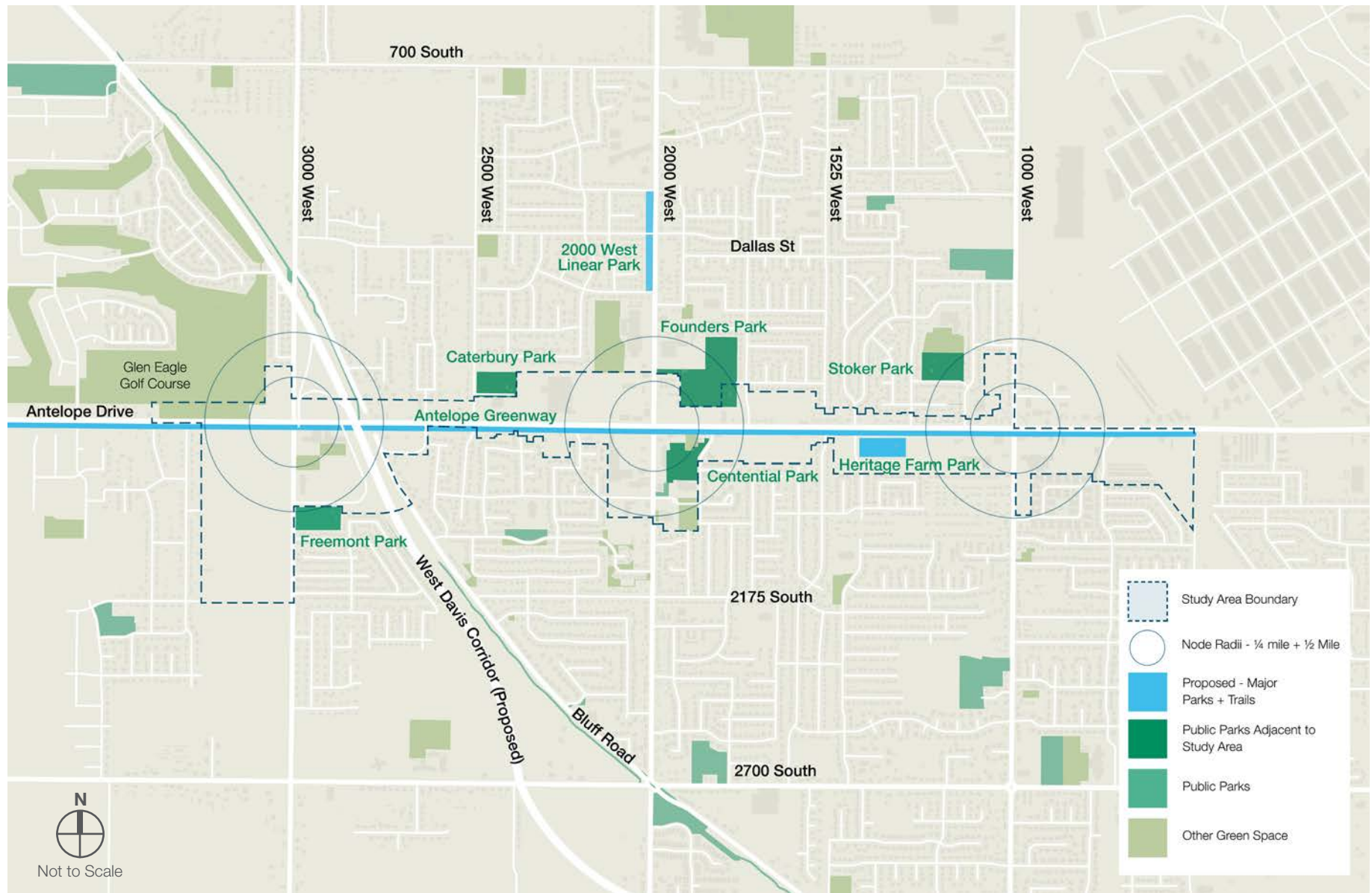


Centennial Park



Rendering for "Syracuse Island" at Centennial Park





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## Land-Use

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### OVERVIEW + OBJECTIVE

The existing zoning in the study area is largely segregated into separate land-uses. The prominent uses include Commercial, low to medium density residential, and agricultural.

The future land use in the Antelope corridor is intended to have a variety of uses within the same zone/area. Therefore, most of the future zoning should be designated for mixed-use. However, there are some exceptions where singling out a specific use in a area will help achieve the vision for the individual nodes.

The concept development plans for each node that are shown in this document establish the foundation for the character creating uses that will make each node unique.

This is a general overview of the proposed Land Uses throughout the corridor. Further detail is provided in the Concept Plan section.



Theater



Restaurant

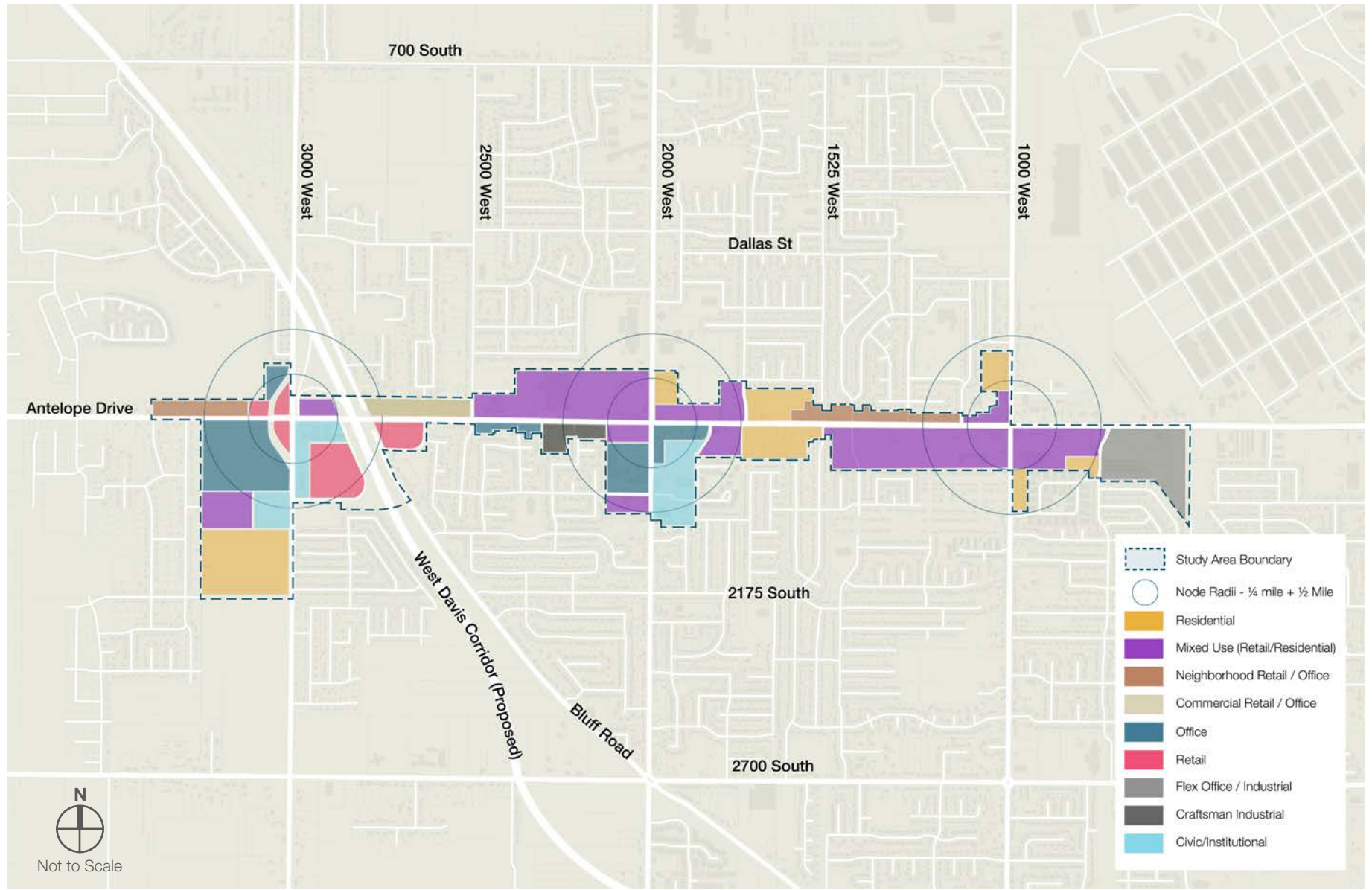


Syracuse City Hall



Town Center Clock + Plaza











# CONCEPT PLAN + ECONOMICS

## Population Impacts

### POPULATION PROJECTIONS

Syracuse is growing! The Governor's Office of Management and Budget projects that Syracuse will double in population by 2060, going from the 2014 estimate of 26,639 to 53,389. Therefore, Syracuse needs to consider adding 26,750 people in the next 40 years.

The question is, where should that growth go? Syracuse does have some undeveloped agricultural land within its boundaries that could accept more housing. However, if all available land was developed based on current development patterns for single family residential, the amount of housing provided would be well short of the projected growth.

Therefore, the concept plan shown in this section takes a different approach to housing development than has traditionally been done in Syracuse. It adds density into the development nodes, with minimal impacts to the existing residential neighborhoods and current way of life, and also preserving undeveloped open space and agricultural land. For this study, building heights are shown at 4 stories and under.

### GROWTH ACCOMMODATION

This plan represents an additional 1,831 units through a healthy mix of townhouses, condos, and stacked-flat multi-family units. Using average household sizes, this number of units could accommodate between 4,834 (national average of 2.64 people per household) and 6,647 (Utah average of 3.63 people per household) people. That means this concept development plan, despite being relatively aggressive for **Syracuse, would still only accommodate 18% - 25% of projected growth.**

Therefore, higher density development appears to be appropriate, and even necessary, to accommodate the region's growing population. This illustrates the importance of planning for ways to implement higher-density housing into the City in a manner that will not become a burden to the City, but instead will create thriving mixed-use centers that will be beneficial for all Syracuse residents. With careful planning and a focus on density and appropriate uses along major corridors, Syracuse can maintain its more rural lifestyle and plan for population growth.





## Economic Impacts

### ECONOMIC ANALYSIS

The Antelope Drive Corridor is a very important corridor for the City’s tax base and economic sustainability, as businesses in the study area account for approximately 70% of the City’s sales tax revenues.

The existing Town Center is currently a significant contributor to the overall sales for the City. The East Gateway Node, anchored by the Smith’s grocery store, also accounts for a significant portion of the City’s sales. Currently, the West Gateway Node does not contribute much tax revenue to the City.

However, the West Davis Corridor will likely change traffic patterns, and could potentially have a significant impact on these nodes by routing much more traffic to the West Gateway, while decreasing traffic at the East Gateway.

One area of concern is the City’s reliance on a few large retailers for the majority of sales tax revenue. For example, RC Willey and Wal-Mart alone account for 61% of total sales citywide. This is equal to roughly \$510,000 in sales tax direct to Syracuse alone.

The concept development plan encourages a diversified and resilient tax base that is not as reliant on a few entities.

### RETAIL CAPTURE RATES

Syracuse has the ability to capture more retail spending within the City, as it currently is capturing only 48% of retail spending. This low number is consistent with many suburban communities without major commercial centers. However, it does demonstrate the ability it has for Syracuse to capture more retail spending.

Sales leakage data shows potential for:

- › Automobile parts and services - These uses are very strong in the current market.
- › Restaurants - The City currently captures a very low percentage of available retail dollars for restaurant use. This suggests possible opportunities for this type.
- › Hotel and accommodations
- › Sporting goods - These are healthy in the current market, particularly ones that offer experiences.

### TAX REVENUES

Additional development, as proposed within this plan, will have the potential to greatly increase Syracuse’s revenues from both property taxes and sales taxes. Currently vacant or agricultural land is generating limited property

taxes, and highest and best use conclusions show that these properties, along with numerous other developed properties, are not reaching their full potential. Consequently, Syracuse is missing out on potential tax dollars. As these properties are developed over time, they will additionally have the effect of improving property values on surrounding sites. The data in the associated table highlights the value

of the proposed buildouts, in 2018 dollars. Future values are assumed to be significantly higher. Furthermore, the table indicates the property tax and sales tax revenues that will be attributed to the city directly (additional taxes will be received by the school district, water district, and other taxing entities). Additional taxes from municipal energy fees and possible road funds would be anticipated to be received by Syracuse.

Assessed Property Value	
2017 Value	\$240,676,056
25 Year Buildout Projected Value	\$551,240,000
Total Increase 129%	
Property Tax	
2017 Tax	\$378,583
25 Year Buildout Projected Tax	\$1,034,373
Total Increase 173%	
Sales Tax	
2017 Tax	\$595,553
25 Year Buildout Projected Tax	\$903,203
Total Increase 51%	

## Market Study

### OVERVIEW + OBJECTIVE

A market study was performed to understand the realistic development opportunities in Syracuse.

This market study represents current market conditions and was used to inform the concept development plan in this section. However, the concept development plan represents a long-term, 25 year build-out. Therefore, it's understood that market conditions will likely change over time as the larger economy changes, growth continues in the City and region, and significant projects occur (such as the West Davis Corridor).

### INVESTMENT CONDITIONS

The table below shows the current investment conditions in Syracuse. Generally, the market for development is strong. Specifically, the market for apartments is very strong, which is consistent with trends across the Wasatch Front. Townhouses are also strong in the current market.

Other use types with high feasibility include flex office and medical office. Hotel and office are moderate. However, the completion of the West Davis Corridor will likely boost feasibility for those uses. General retail development is currently the most tenuous, though some specific uses such as restaurants and destination based retail are feasible.

### TOWN CENTER RECOMMENDATIONS

Retail across the Wasatch Front is changing as consumers and businesses adapt to online shopping habits and other changes in the industry. Successful centers are showing an ability to attract people at all times of the day, particularly by having entertainment options and/or experiential retail options. Consumers are becoming more desirous to “experience” a product prior to purchasing, thus providing a potential benefit to brick and mortar stores over online options. Some of these uses are:

- › Entertainment driven options
- › Destination based retail
- › Gathering places

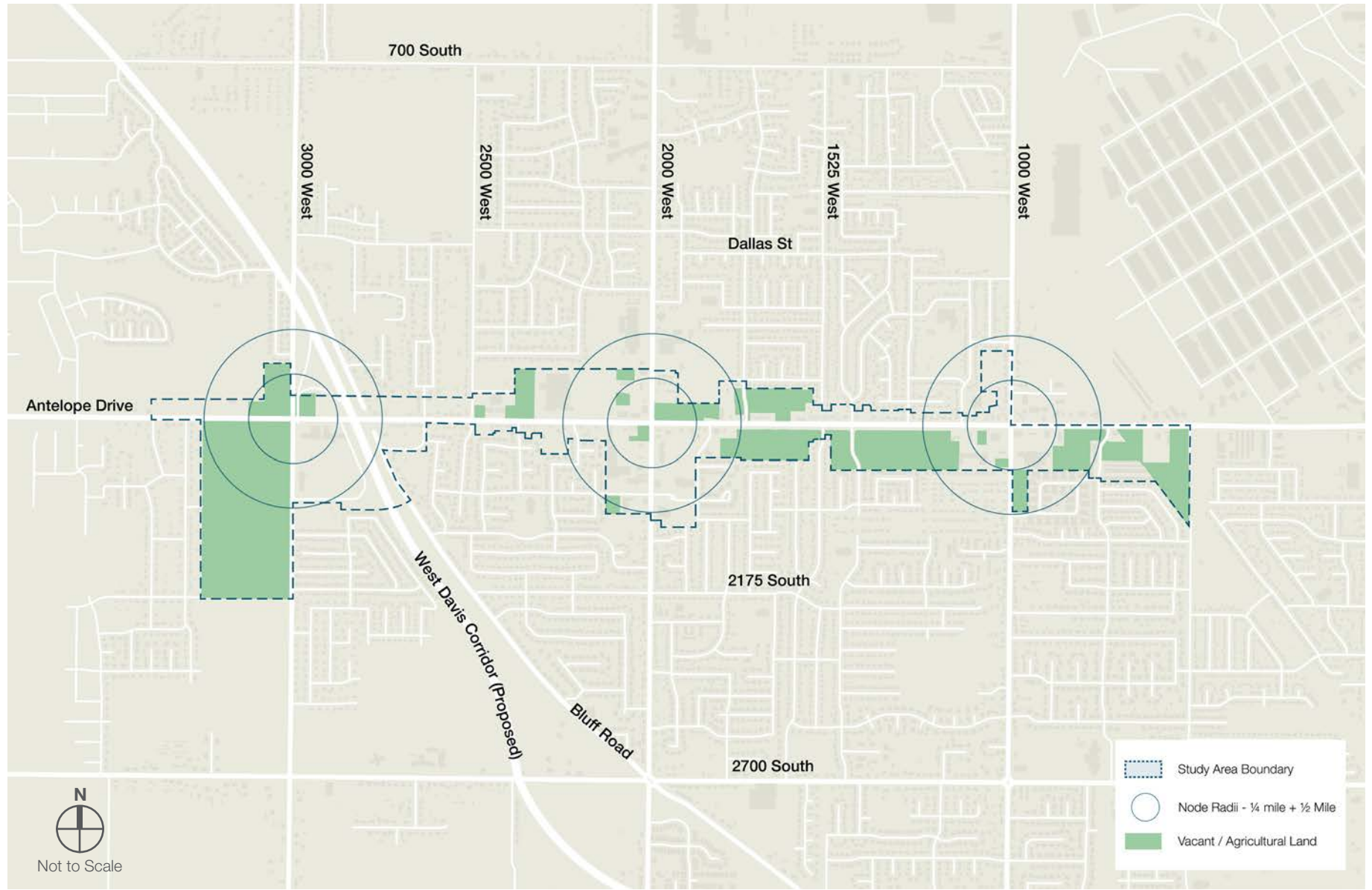
### GENERAL RECOMMENDATIONS

Key steps to maximize development potential in the study area in the near term include:

- › Including the west side in a CRA.
- › Educate property owners on possible available incentives with a CRA.
- › Focus near-term on key sites with low improvement values.
- › Utilize density within the nodes to enhance financial viability.
- › Recognize the impact on sales tax revenue that different retailers provide and be proactive with incentivizing retailers with high tax benefits.

Use Type	FV - 10,000 sf	Projected Value	Feasible?	Capitalization Rates	Investment Conditions
Apartments	\$150.00 sf	\$115.00 sf	Yes	6.0%	Very Strong
Townhomes	\$125.00 - \$150.00	\$100.00 sf	Yes	NA	Strong
Retail	\$160.00 sf	\$155.00 sf	Yes	8.0%	Tenuous
Office	\$170.00 sf	\$170.00 sf	Possible w/ Incentives	7.5%	Moderate-to-Strong
Flex	\$145.00 sf	\$125.00 sf	Yes	7.5%	Strong
Medical Office	\$230.00 sf	\$200.00 sf	Yes	7.5%	Strong
Hotel	\$220.00 sf	\$220.00 sf	Potentially	8.0%	Moderate





## Concept Development Plan

### INTENT

This concept plan has been developed to show a long-term potential buildout in the study area over the next 25 years. The concept development shown in the plan is based on the market analysis, city planning best practices, and input from City leadership and development stakeholders.

The intent of this plan is to show a hypothetical approach to development along the Antelope Drive Corridor in order to understand the capacity that this area has to accommodate future growth. This information should be used to inform decisions and guide expectations. The concepts do not reflect what the finalized form should be, but rather, these are sketches to illustrate possibilities.

The development of the Antelope Drive Corridor is clustered into three nodes at major intersections, with the intention that each node develops its own unique character and vision with a distinct mix of land-uses.

In addition, an economic analysis for this potential development has been performed to understand what the long-term economic impacts to the City are for different development scenarios.

As this is a long-term plan, these concepts and any future development are heavily influenced by complex economic realities, market demands in addition to public input, city leadership and development stakeholders.

Generally, future development is intended to be infill uses that will help create a walkable neighborhood/town center with buildings that address the street and add interest that encourage visitation leading to a critical mass of density needed to create a “place.”

This section goes into more detail for each node and outlines the vision, land-uses, building heights, and place types that combine to give each node a unique character and purpose.

### East Gateway



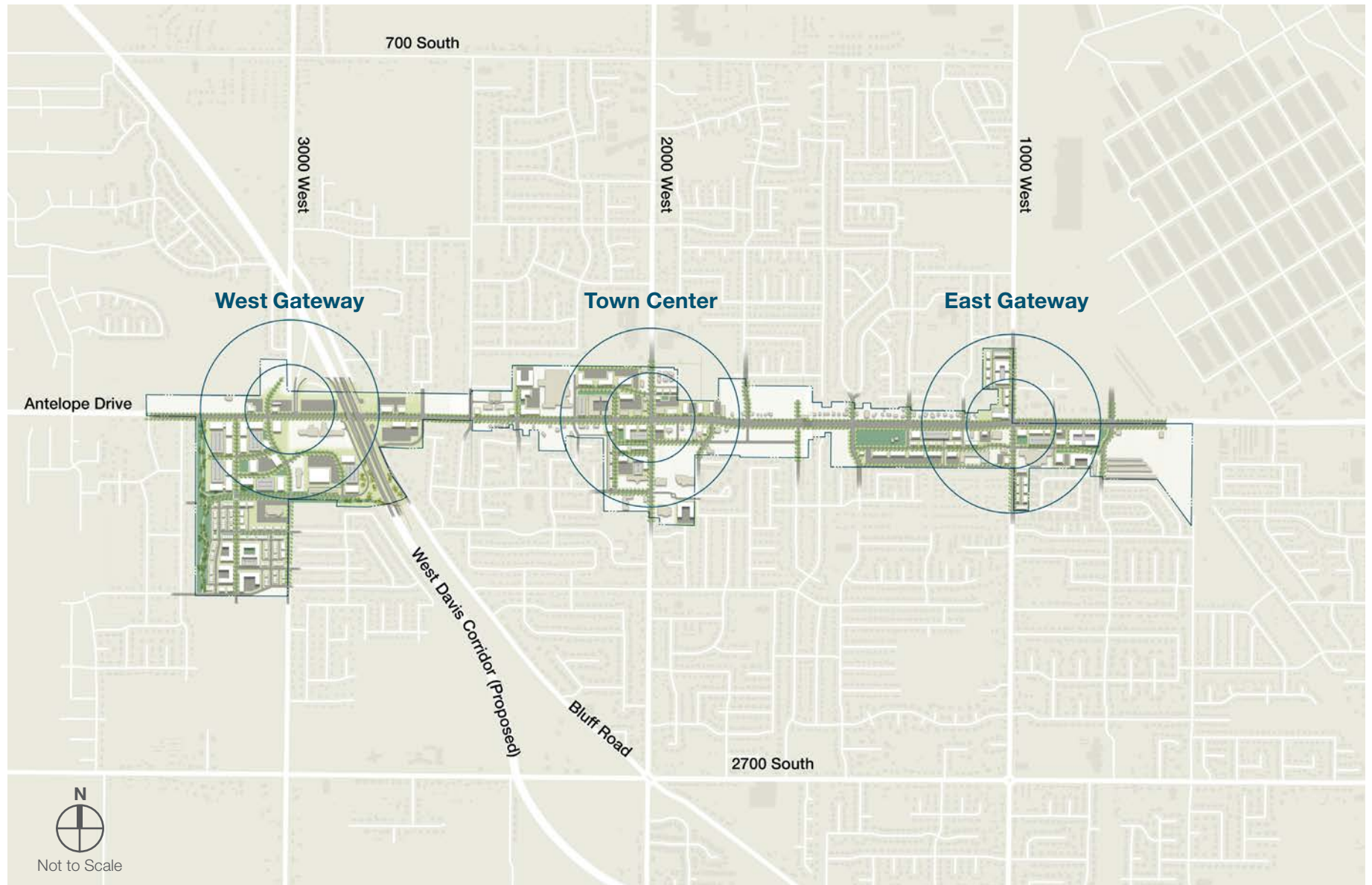
### Town Center



### West Gateway







## East Gateway Node 1 (1000 West)

### VISION

The East Gateway node is envisioned to be a small village center that will define the eastern boundary of Syracuse and become a gateway into the City.

Currently, this node is the major gateway to the City. However, the traffic volumes on this section of Antelope Drive are actually expected to be reduced once the West Davis Corridor is completed. This therefore creates the ability for this node to become less about serving vehicles passing through, and allows for a walkable neighborhood with that focuses more on serving local Syracuse Residents to be created.

Some of the amenities this node will provide are a grocery store, heritage farm park, and a farmers market.





## East Gateway - Place Types




### VISION

The place types described on this page are appropriate for the East Gateway Node.

The heart of the East Gateway is a Mixed-Use Neighborhood, where a combination of residential, retail, and office creates a walkable and vibrant neighborhood.

The east edge of this node is appropriate for flex office and industrial uses, as the area is currently characterized by industrial and storage facilities. It also contains a large easement for power lines.

A transition neighborhood use is appropriate as a transition between existing neighborhoods and new development.

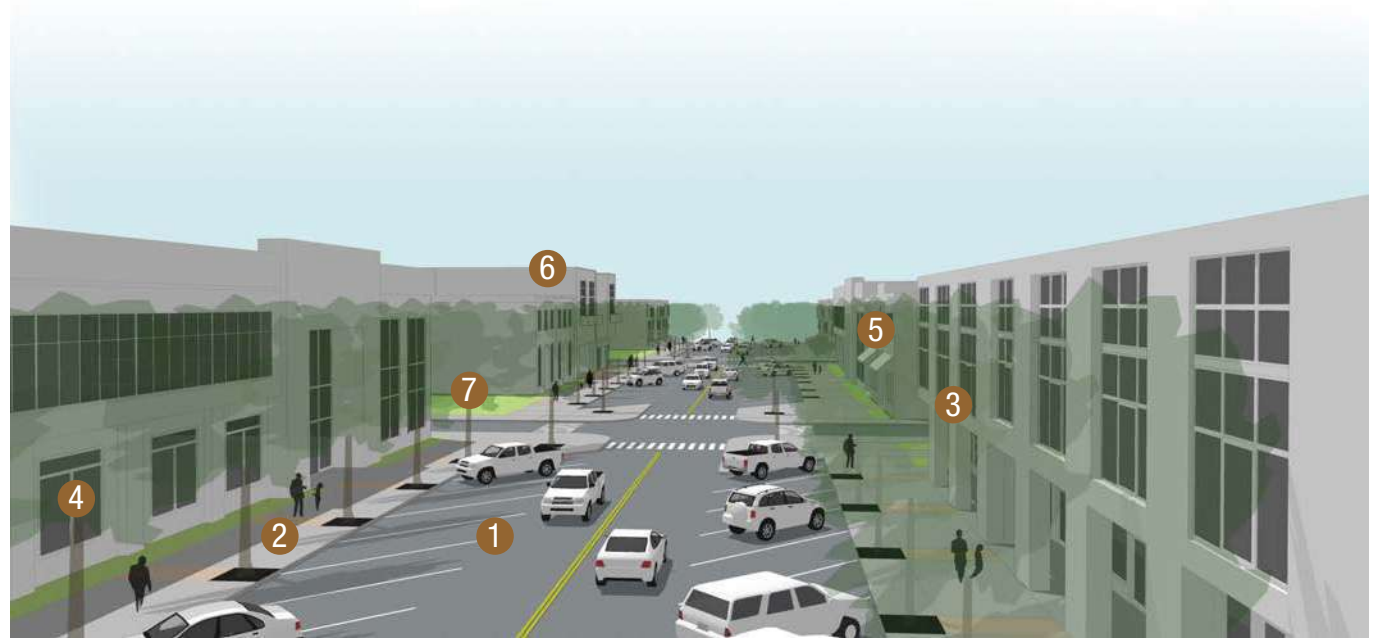
East Gateway Place Types	Images	Description
Mixed-Use Neighborhood		Offers residents the ability to live, shop, work, and play in one community. They include a mixture of housing types and residential densities within close proximity to the goods and services residents need on a daily basis. A small urban square may support commercial uses in the neighborhood. The design and scale of the development in a mixed-use neighborhood encourages active living, with a complete and comprehensive network of walkable streets.
Flex Office / Industrial Park		Provides basic jobs and keeps people in the city during normal work hours. They allow occupants the flexibility of alternative uses of the space and typically locate near major transportation corridors. Flex office may include light manufacturing centers, warehouse, showrooms, transportation hubs, service center or technology centers.
Transition Neighborhood		Supports a mix of moderate density housing options. These neighborhoods are more compact and walkable. The design and scale of the development in an urban transition neighborhood encourages active living and access to a more complete and comprehensive mobility network.

## East Gateway Plan

### LAYOUT AND ARRANGEMENT

The graphic on this page shows a conceptual depiction of the retail street, which is envisioned as a new street to the south of Antelope Drive. This graphic demonstrates a number of urban design guidelines that combine to create a walkable and vibrant street and neighborhood.

The plan on the following page represents the concept layout and arrangement of streets, parking, buildings, and open space, which combined, demonstrate the physical expression for the vision for the East Gateway Node.



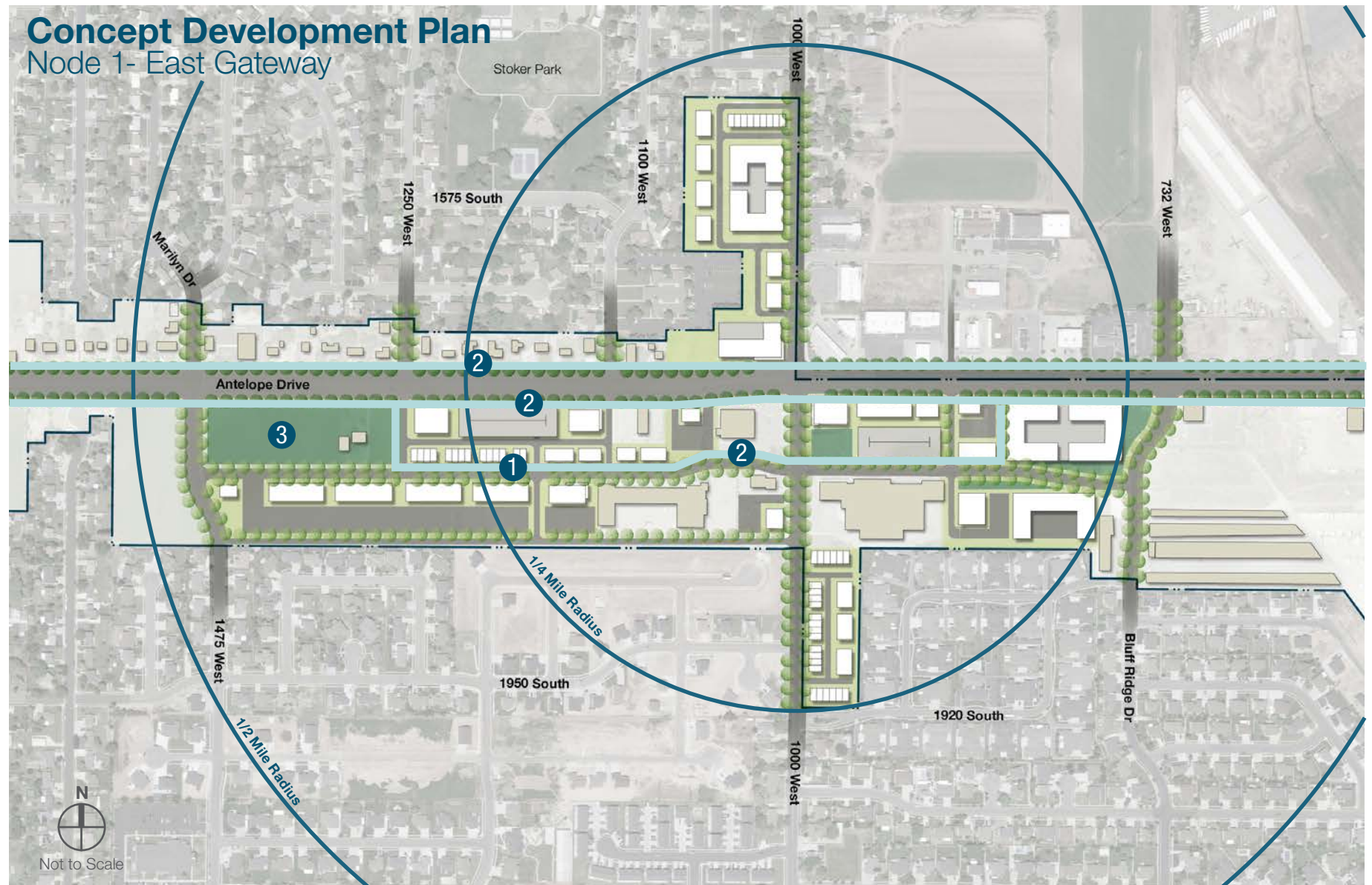
### 3D GRAPHIC ELEMENTS

- 1 Street calming features such as narrow lane widths, bulb-outs, crosswalks, street trees and angled parking.
- 2 Walkable streetscape features such as canopy of street trees, unique paving, generous pedestrian space.
- 3 Human scaled buildings and street on both side of street create compression for comfortable pedestrian experience.
- 4 Transparency on ground-floor of buildings.
- 5 Buildings address the street with windows, entries, storefronts, and residential stoops.
- 6 Building heights vary between 2-4 stories.
- 7 Strategically located open space provides community gathering space and/or quiet respite.

### PLAN ELEMENTS

- 1 Walkable retail and residential street lined with human scaled buildings, storefronts, and residential stoops.
- 2 Greenway / multi-use path along Antelope Drive with additional route through retail street.
- 3 Existing agricultural field to be preserved as a public open space with heritage themed park.





## East Gateway - Land-Use

### INTENT

The land-uses in the East Gateway node are intended to be a mix of uses in order to create a true mixed-use neighborhood. Currently, the majority of land-uses in the area are retail and service uses. In order to complete the neighborhood, office and residential uses should be added, as well as parks and other public open spaces.

Buildings are encouraged to be vertically mixed-use, wherever feasible, with ground floor retail on office and residential buildings. However, focus should be on lining the Retail Street, first and foremost, with Antelope Drive also a secondary priority for creating a walkable retail street.

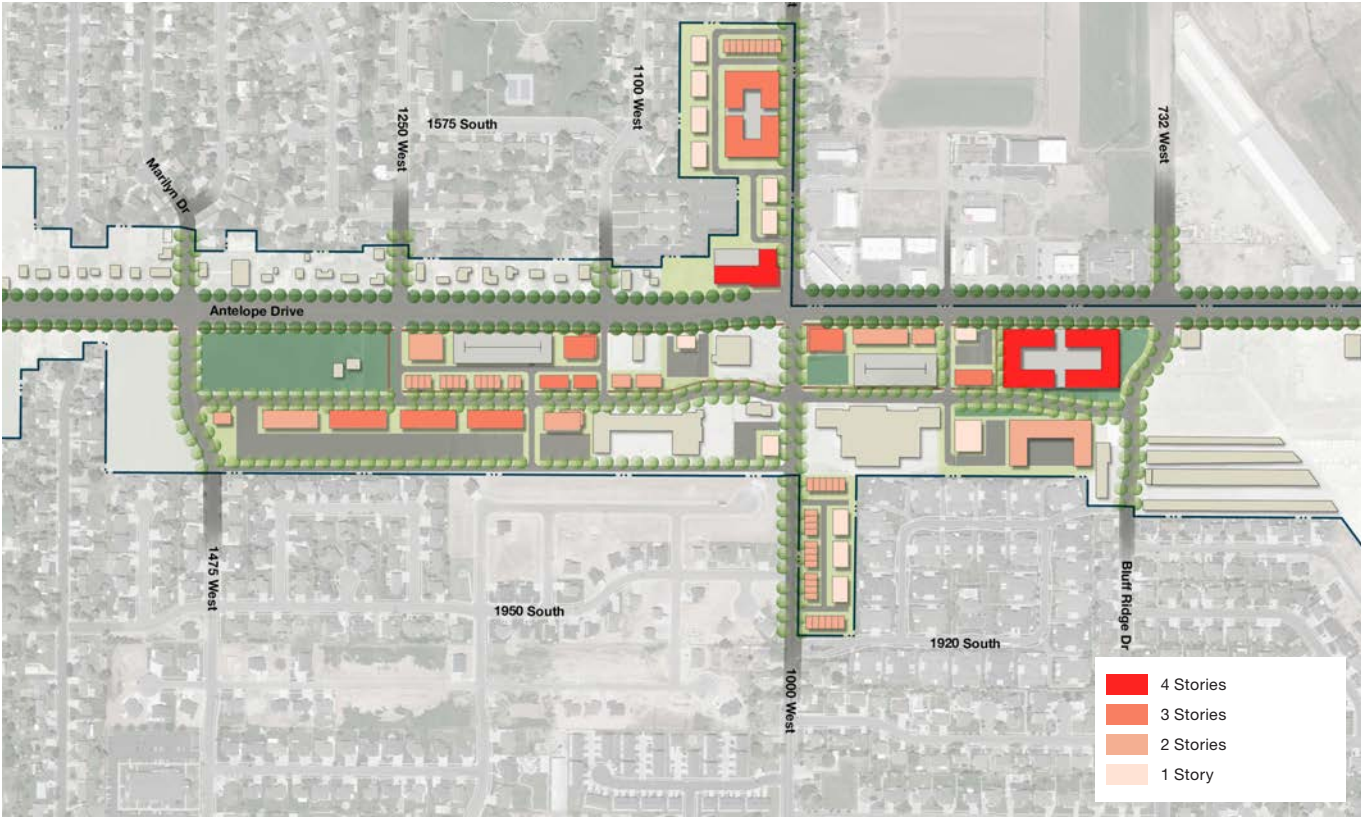




# East Gateway - Building Heights

## INTENT

The plan graphic on this page shows conceptual building heights. A “step-down” approach for building heights should be used, with buildings near major roads and intersections taller, and stepping down as they get closer to existing neighborhoods.



## NUMBERS BREAKDOWN

This table shows a numbers breakdown for the East Gateway Node. These numbers reflect new buildings depicted on the concept development plan.

Use Type	Square Feet / Units	Projected Value	Property Tax to Syracuse**	Sales Tax to Syracuse	Total Tax Revenue
Townhomes	47 units	\$13,200,000	\$11,400	NA	\$11,400
Quad Homes	36 units	\$10,100,000	\$8,750	NA	\$8,750
Stacked Flats	511 units	\$79,000,000	\$68,300	NA	\$68,300
Office	148,000 square feet	\$25,200,000	\$39,640	NA	\$39,640
Dining	15,500 square feet	\$4,650,000	\$7,300	\$31,000	\$38,300
Retail	61,000 square feet	\$9,760,000	\$15,350	\$76,250	\$91,600
Total		\$141,910,000	\$150,740	\$107,250	\$257,990

## Town Center

### *Node 2 (2000 West)*

#### VISION

This Node is currently identified as Syracuse's Town Center, and already contains some development consistent with the vision for the town center, including restaurants and entertainment.

The Town Center is the heart of culture and activity in Syracuse City. This node contains the main commercial and retail uses in the City, as well as being the City's Civic Center. The node will contain Centennial Park and Founder's Park, which are the City's signature open spaces. It will become the City's living room.

While this node is intended to serve as the town center for Syracuse residents, it is expected that it will become a regional draw due to its active, vibrant, and exciting nature. It will also become a prime destination for visitors to Antelope Island to stop for restaurants, supplies, hospitality, and more.








## Town Center - Place Types

### VISION

The place types described on this page are appropriate for the Town Center Node.

As the heart of the City, the Town Center place type is appropriate in this node. It may be supplemented on the periphery by a mixed-use neighborhood.

A transition neighborhood use is appropriate as a transition between existing neighborhoods and new development.

Town Center Place Types	Images	Description
Town Center		Town centers are locally-serving areas of economic, entertainment, and community activity. The size of a town center makes it an employment center and shopping destination for surrounding neighborhoods. Buildings typically stand two or more stories with condominiums or apartments over storefronts. Public open space often helps to organize and support commercial uses. The design and scale encourages active living, with a comprehensive and interconnected network of walkable streets.
Mixed-Use Neighborhood		Offers residents the ability to live, shop, work, and play in one community. They include a mixture of housing types and residential densities within close proximity to the goods and services residents need on a daily basis. A small urban square may support commercial uses in the neighborhood. The design and scale of the development in a mixed-use neighborhood encourages active living, with a complete and comprehensive network of walkable streets.
Transition Neighborhood		Supports a mix of moderate density housing options. These neighborhoods are more compact and walkable. The design and scale of the development in an urban transition neighborhood encourages active living and access to a more complete and comprehensive mobility network.

## Town Center Plan

### LAYOUT AND ARRANGEMENT

The graphic on this page shows a conceptual depiction of Antelope Drive. This graphic demonstrates a number of urban design guidelines that combine to create a walkable and vibrant street and neighborhood.

The plan on the following page represents the concept layout and arrangement of streets, parking, buildings, and open space, which combined, demonstrate the physical expression for the vision for the Town Center Node.



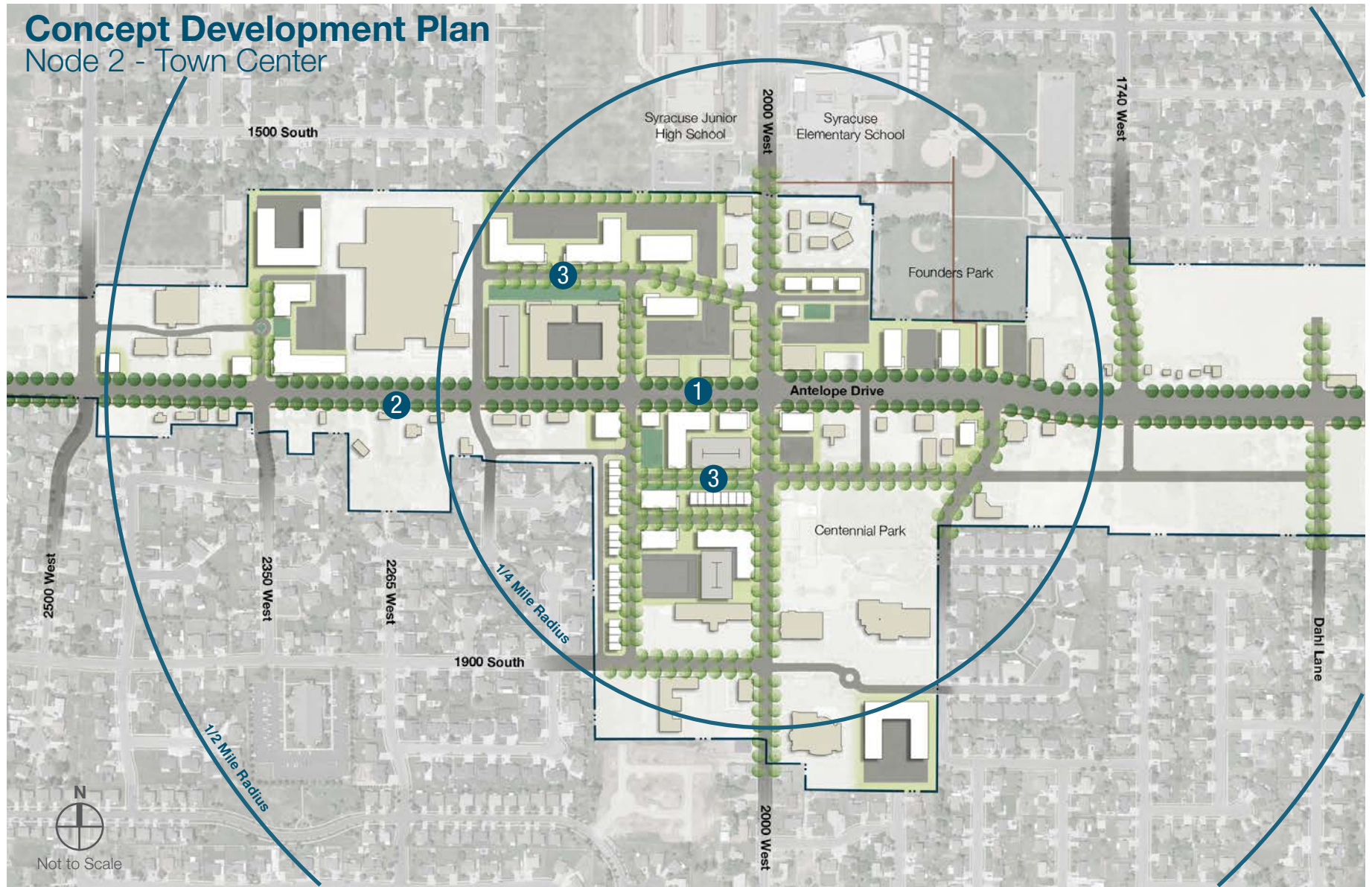
### 3D GRAPHIC ELEMENTS

- 1 Reconfigured street maintains all vehicle lanes and capacity in the current public right-of-way. However, additional facilities are added to increase mobility and safety, including painted bike lanes and raised (curb-level) multi-use pathway.
- 2 Calming features such as narrow lane widths, bulb-outs, crosswalks, street trees and parallel parking.
- 3 Unique landscaping and landscape features provide character and a distinct look at feel that will help brand the City.
- 4 Walkable streetscape features such as canopy of street trees and generous pedestrian space.
- 5 Human scaled buildings and street on both side of street create compression for comfortable pedestrian experience.
- 6 Transparency on ground-floor of buildings.
- 7 Buildings address the street with windows, entries, storefronts, and residential stoops.
- 8 Building heights vary between 2-4 stories.

### PLAN ELEMENTS

- 1 Consistent, human scaled buildings front Antelope Drive, making Antelope Drive the primary commercial / retail street.
- 2 Greenway / multi-use path along Antelope Drive, with connections to the major parks.
- 3 Additional streets created within existing street network to create a finer grained street network.



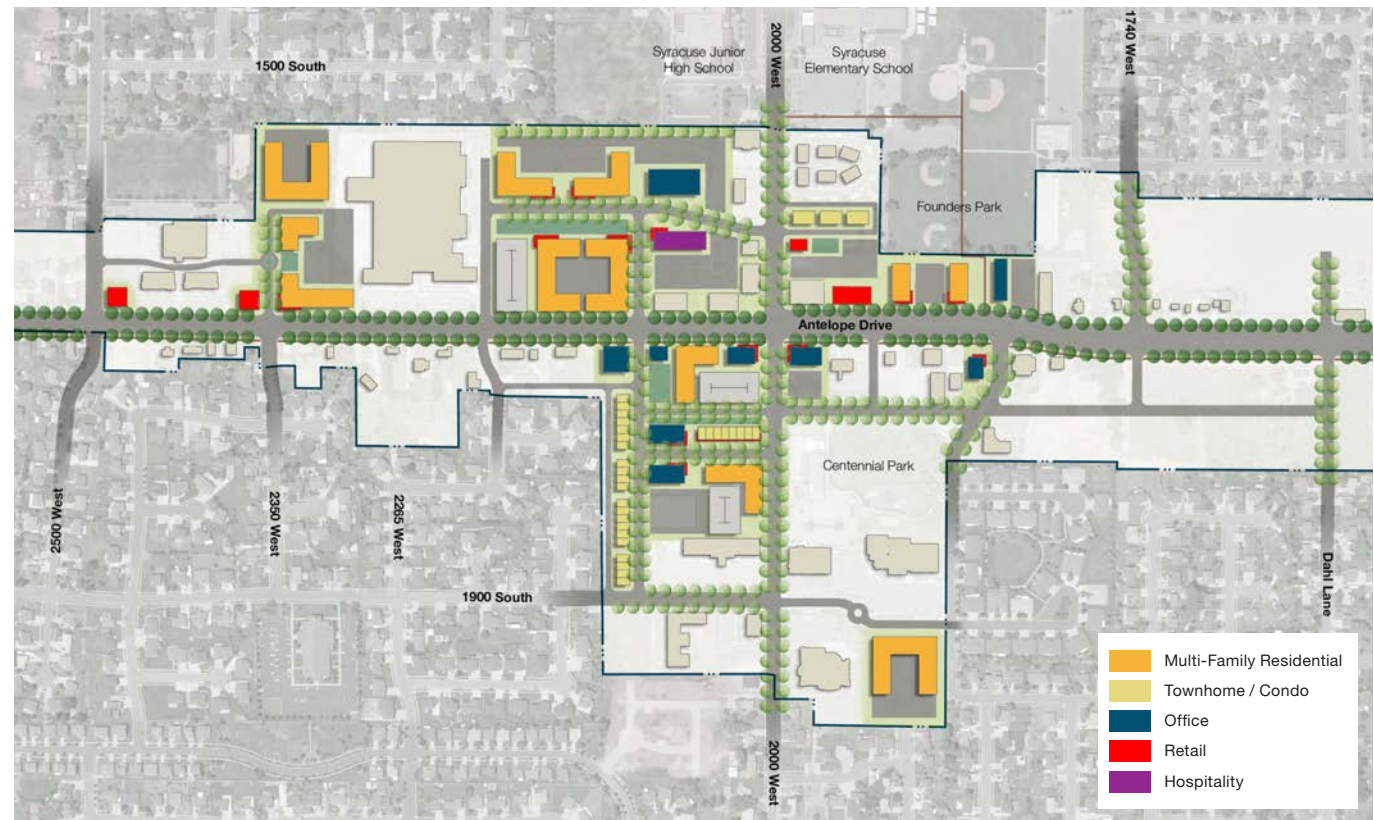


## Town Center - Land-Use

### INTENT

The land-uses in this node integrate both the Town Center and Mixed-Use place types together in order to synergize the energies of both. This creates a place where professional offices can tuck next to commercial retail and restaurants, and adjacent to open space. This is not just a commercial center, but a livable place where you can eat your lunch or walk your dog.

Buildings are encouraged to be vertically mixed-use, wherever feasible, with ground floor retail on office and residential buildings. However, focus should be on lining the Antelope Drive, first and foremost, with the newly created internal streets becoming a secondary priority for creating a walkable retail street.





# Town Center - Building Heights

## INTENT

The plan graphic on this page shows conceptual building heights. A “step-down” approach for building heights should be used, with buildings near major roads and intersections taller, and stepping down as they get closer to existing neighborhoods.



## NUMBERS BREAKDOWN

This table shows a numbers breakdown for the Town Center Node. These numbers reflect new buildings depicted on the concept development plan.

Use Type	Square Feet / Units	Projected Value	Property Tax to Syracuse**	Sales Tax to Syracuse	Total Tax Revenue
Townhomes	47 units	\$13,200,000	\$11,400	NA	\$11,400
Quad Homes	36 units	\$10,100,000	\$8,750	NA	\$8,750
Stacked Flats	511 units	\$79,000,000	\$68,300	NA	\$68,300
Office	148,000 square feet	\$25,200,000	\$39,640	NA	\$39,640
Dining	15,500 square feet	\$4,650,000	\$7,300	\$31,000	\$38,300
Retail	61,000 square feet	\$9,760,000	\$15,350	\$76,250	\$91,600
Total		\$141,910,000	\$150,740	\$107,250	\$257,990

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## West Gateway *Node 3 (3000 West)*

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### VISION

The West Gateway Node will become a regional commercial center that supports the community economic goals and embraces the future West Davis Corridor. With the completion of the West Davis Corridor, It will become the new “Gateway to Antelope Island”, and give this node the ability to have the greatest impact on realizing the goals of increasing daytime population and capitalizing on tourism.

It will connect to the existing neighborhoods through improved circulation to schools, church through bike trails and improved streets.





## West Gateway - Place Types




### VISION

The place types described on this page are appropriate for the West Gateway Node.

The most significant place type in the West Gateway is a Regional Commerce Center. The confluence of the future West Davis Corridor and the amount of undeveloped land create a great opportunity to create a regional destination centered around office and other day time uses.

A mixed-use neighborhood should support the regional commerce center by providing retail and residential nearby.

A transition neighborhood use is appropriate as a transition between existing neighborhoods and new development.

	Town Center Place Types	Images	Description
	Regional Commerce Center		<p>Emerging mixed-use center planned or developed with large-scale master plans. Provides service jobs in the region and have a greater density of employees. Typical uses include professional offices, corporate campus, research and development and technology centers. Regional centers include a variety of housing types and densities, as well as having a support retail component that serves the center, such as restaurant and hospitality uses.</p>
	Mixed-Use Neighborhood		<p>Offers residents the ability to live, shop, work, and play in one community. They include a mixture of housing types and residential densities within close proximity to the goods and services residents need on a daily basis. A small urban square may supports commercial uses in the neighborhood. The design and scale of the development in a mixed-use neighborhood encourages active living, with a complete and comprehensive network of walkable streets.</p>
	Transition Neighborhood		<p>Supports a mix of moderate density housing options. These neighborhoods are more compact and walkable. The design and scale of the development in an urban transition neighborhood encourages active living and access to a more complete and comprehensive mobility network.</p>

## West Gateway Plan

### LAYOUT AND ARRANGEMENT

The graphic on this page shows a conceptual depiction of the local office street, which is envisioned as a new street connecting to Antelope Drive. This graphic demonstrates a number of urban design guidelines that combine to create a walkable and vibrant street and neighborhood.

The plan on the following page represents the concept layout and arrangement of streets, parking, buildings, and open space, which combined, demonstrate the physical expression for the vision for the West Gateway Node.



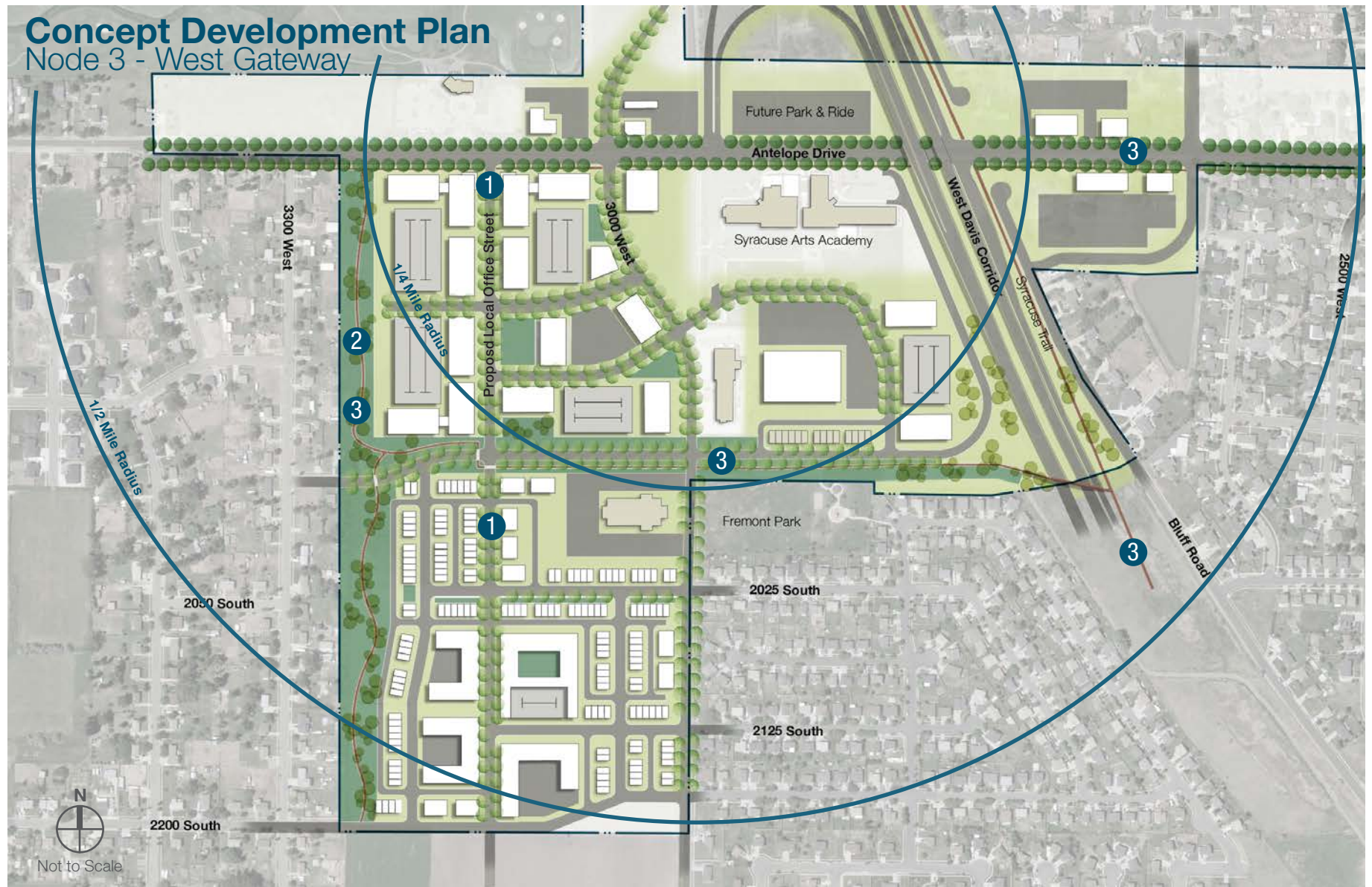
### 3D GRAPHIC ELEMENTS

- 1 Street Calming features such as narrow lane widths, bulb-outs, crosswalks, street trees and angled parking.
- 2 Walkable streetscape features such as canopy of street trees, unique paving, generous pedestrian space.
- 3 Protected Cycle Track provides safe and convenient facilities for cyclists and enhances mobility.
- 4 Human scaled buildings and street on both side of street create compression for comfortable pedestrian experience.
- 5 Parking Structure consolidates to smaller footprint parking behind buildings.
- 6 Transparency on ground-floor of buildings.
- 7 Buildings address the street with windows, entries, and storefronts.
- 8 Building heights vary between 2-5 stories.
- 9 Strategically located open space provides community gathering space and/or quiet respite.

### PLAN ELEMENTS

- 1 Office buildings are organized along the street and surround a shared parking structure.
- 2 Green Buffer between new development and existing neighborhood.
- 3 Greenway / multi-use path along Antelope Drive connecting with West Davis Corridor Trail and trail within the Green Buffer area between future office and existing residential.





## West Gateway - Land-Use

### INTENT

The most significant land-use in this node is the office use that is the heart of the Regional Commerce Center. The uses in this node also allow for a mid-box or big box retailer, as well as other retail.

The southern portion of this node is largely dedicated to residential, with some larger multi-family residential buildings surrounded by moderately dense townhouse and condo buildings.

A hotel is proposed near the West Davis Corridor to capitalize on the visibility and support Syracuse's goal of promoting tourism at Antelope Island.

The concept for the West Gateway allows for a phased approach to realize growth over time. Master planned development will organize structures to accommodate current and future demand.





# West Gateway - Building Heights

## INTENT

The plan graphic on this page shows conceptual building heights. A “step-down” approach for building heights should be used, with buildings near major roads and intersections taller, and stepping down as they get closer to existing neighborhoods.



## NUMBERS BREAKDOWN

This table shows a numbers breakdown for the West Gateway Node. These numbers reflect new buildings depicted on the concept development plan.

Use Type	Square Feet / Units	Projected Value	Property Tax to Syracuse**	Sales Tax to Syracuse	Total Tax Revenue
Townhomes	148 units	\$22,950,000	\$19,900	NA	\$19,900
Quad Homes	36 units	\$5,600,000	\$4,850	NA	\$4,850
Stacked Flats	358 units	\$55,500,000	\$48,000	NA	\$48,000
Office	914,400 square feet	\$155,500,000	\$245,000	NA	\$245,000
Dining	20,000 square feet	\$6,000,000	\$9,400	\$40,000	\$49,400
Retail	20,500 square feet	\$3,300,000	\$5,200	\$23,000	\$28,200
Total		\$248,850,000	\$332,300	\$63,000	\$395,350







# TRANSPORTATION + MOBILITY

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## Vehicular Transportation

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### OVERVIEW

Over the next few years, two major infrastructure projects are planned within the study area that could potentially impact future ADT volumes and traffic operations. These projects include the West Davis Corridor and the widening of Antelope Drive. To better understand the impacts of these projects, future 2040 ADT volumes were also collected and analyzed.

### WEST DAVIS CORRIDOR

The West Davis Corridor project proposes to add a new 19-mile freeway that would travel through Davis and Weber counties and would connect the Farmington and West Haven. The new freeway would help address the anticipated growth in population and employment in both Davis and Weber Counties and the resulting demand on the region's transportation network. The West Davis Corridor project also proposes the construction of a 20-mile continuous trail network for bicyclists and pedestrians parallel to the freeway corridor.

### ANTELOPE DRIVE ROAD WIDENING

In addition to the West Davis Corridor project, major infrastructure projects planned for the study area also includes the widening of Antelope Drive west of 2000 West. Per review of the 2015-2040 Wasatch Front Regional Council (WFRC) Regional Transportation Plan, Antelope Drive is expected to be widened from a two lane collector to a four lane major arterial.

### EXISTING CONDITIONS

Results from the analysis show that a majority of the street segments within the study area are currently operating at acceptable levels (LOS C or better), with a few segments operating at unacceptable levels (LOS D or worse). Subsequent to the construction of the West Davis Corridor project, ADT volumes along Antelope Drive are expected to decrease in 2040. Additionally, the expansion of Antelope Drive from a two lane collector to a four lane arterial will ease congestion and improve traffic operations from unacceptable to acceptable levels.

### FUTURE IMPACTS

To better understand the impacts of these projects on travel patterns and traffic operations within the study area, projected 2040 ADT volumes for Antelope Drive were obtained from UDOT. The projected 2040 ADT volumes represent future volumes on Antelope Drive resulting from the construction of the West Davis Corridor project. Results of this analysis indicate significant decreases in ADT volumes along Antelope Drive with the construction of the West Davis Corridor in the year 2040, which could potentially signify a decrease in visitors to the Syracuse

Town Center area. Additionally, the widening of Antelope Drive also helps to improve traffic operations along the corridor, resulting in LOS of C or better.

The expansion of Antelope Drive and the West Davis Corridor project, provides Syracuse City with an opportunity to improve its streetscape design and enhance multi-modal connections.

Existing right-of-way widths allow for the restriping of paved shoulders to accommodate buffered bike lanes and improved pedestrian facilities. These proposed streetscape improvements will improve safety and connectivity to the existing and planned bicycle infrastructure and will also help to promote more biking and walking along the Antelope Drive corridor.





## Complete Streets

### OVERVIEW

As existing streets are redesigned, or new streets are constructed within the study area, they should follow complete streets principles.

### SAFETY

The over-arching goal of complete streets are to increase safety by providing accommodations for all modes of transportation. Smart Growth America defines complete streets as offering “...safe access for all users, including pedestrians, bicyclists, motorists, and transit riders of all ages and abilities. Complete streets make it easier to cross the street, walk to shops, and bicycle to work.”

### STREET NETWORKS

Complete streets should be thought of in terms of both individual streets and overall street networks. Individual streets may or may not provide for all modes of travel. However, the street network for each node, and throughout the corridor, should provide safe bikeways, walkable pedestrian routes, and convenient transit opportunities (existing and future).

Adding streets to the existing street network will help break down large blocks and create a safer, more walkable street network.



Existing Street Network - East Gateway Node



Existing Street Network with Finer-Grained Street Network Added - East Gateway Node



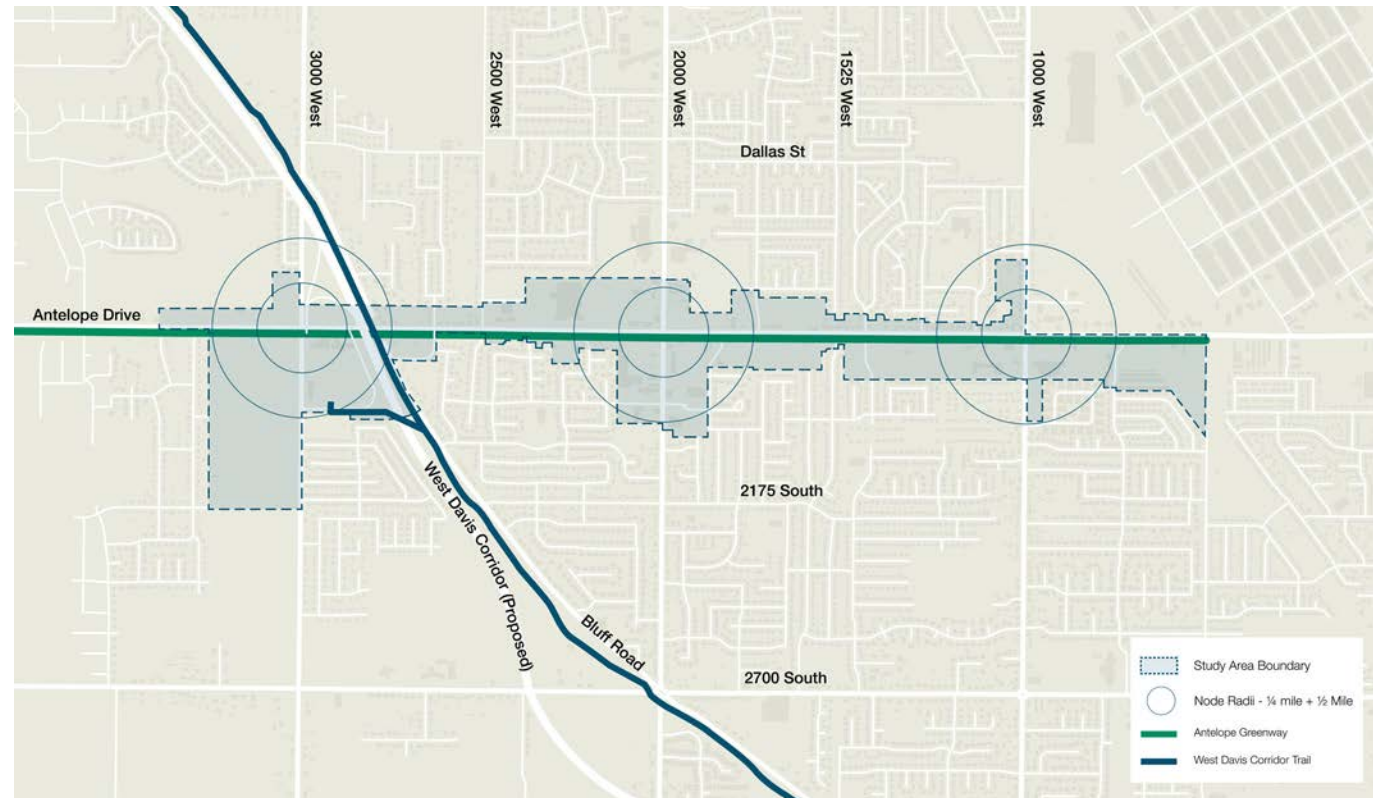


## Antelope Drive Greenway

### OVERVIEW

An important part of this plan is creating a greenway/multi-use path for cyclists and pedestrians along Antelope Drive. This Greenway will run the entire length of Antelope Drive in Syracuse City, connecting the three nodes, as well as providing a direct pedestrian and cycling connection to the Antelope Island Causeway. The Antelope Greenway will also connect to the planned West Davis Corridor Trail.

See page 83 in the Design Guidelines Section for more detail on the size, layout, and character of the Greenway.



## Pedestrian Improvements, Walkability + Safety

### OVERVIEW

Currently, the street network within the study area is predominately designed to accommodate vehicles, and there are a number of large streets that are designed for high vehicle capacities and high speed, but includes very few design accommodations for pedestrians.

### STREET CROSSINGS

One of the biggest current issues facing walkability within the study area is the challenge and risk of crossing major streets, specifically Antelope Drive and 2000 West. Currently, Antelope Drive has crosswalks spaced at approximately 1/2 mile on the east side of 2000 West, and about 1 mile on the west side.

Increasing these crossings is essential for the develop node concept to be successful. At minimum, crossings should happen every 1/4 mile.

### HAWK SIGNALS + TRAFFIC SIGNALS

To facilitate safe pedestrian crossings on major roads, the use of HAWK signals is encouraged. HAWK signals, an acronym for **H**igh-intensity **A**ctivated cross**W**alk, is a traffic control device used to stop road traffic and allow pedestrians to cross safely.

A HAWK signal was recently installed on 2000 West between Syracuse Jr. High and Syracuse Elementary to provide a safer crossing. These use of these signals should be expanded to provide safe crossings at reasonable intervals throughout the study area. The map on the following page show recommended locations for future HAWK signals.

Once the West Davis Corridor is completed, new traffic signals will be added around the on-ramp and off-ramps. These signals should be designed to maximize safe pedestrian crossings.

### PEDESTRIAN CONNECTIONS

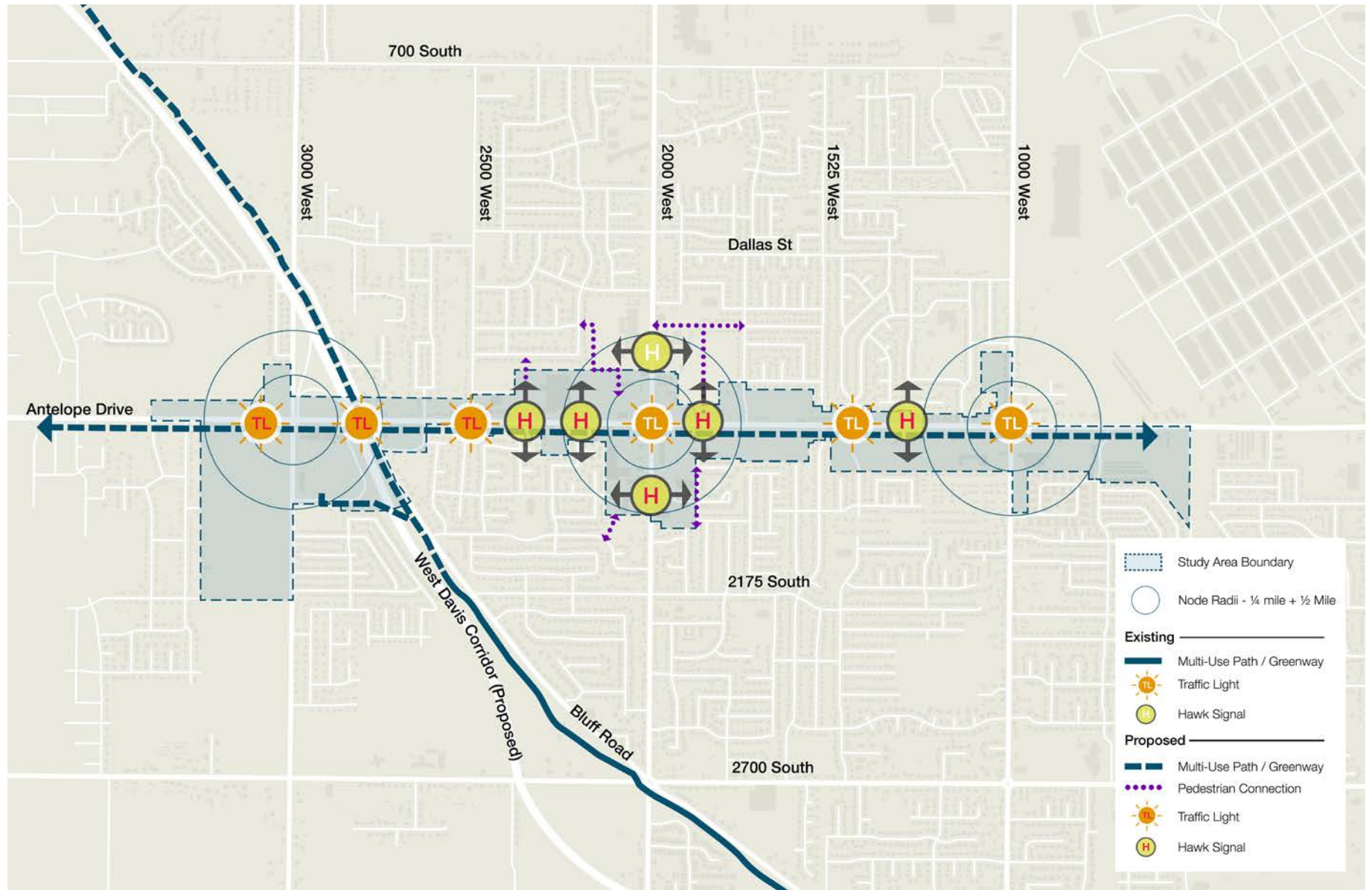
Pedestrian connections should be created and/or enhanced to provide multiple strong connections between the development nodes and the existing neighborhoods. This will reduce the need for residents to drive into the nodes, and will provide additional safe walking routes for residents to get around the City. The map on the following page shows recommended locations for pedestrian connections, but these connections should be considered and created wherever possible.

### MULTI-USE PATHWAYS + GREENWAY

The previous page describes the Antelope Drive Greenway, which will be an important factor in pedestrian connectivity and safety. Connecting to the West Davis Corridor Trail will also provide connections to miles of trails throughout Davis County.







## Bicycle Improvements

### OVERVIEW

The existing bike infrastructure in the study area is limited to on-street bike lanes on Antelope Drive (east of 2000 W) and two other on-street bike lanes on 2500 W and 1475 W that connect with Antelope Drive. See the Syracuse City Trails Master Plan (2012) for planned expansion of the bike network.

### SHARED ROAD BIKEWAY

Shared road bikeways, or “sharrows” are road markings to indicate a shared lane environment for bicycles and automobiles. This treatment should only be used on streets with low speeds (less than 25 mph). This treatment should not be considered a substitute for bike lanes, cycle tracks, or other separation treatments where these types of facilities are otherwise warranted or space permits.

### STRIPED BIKE LANES

Bike lanes designate an exclusive space for bicyclists through the use of pavement markings. When using bike lanes, generous space should be allocated to the bike lane, and where space permits, a designated buffer space separating the bike lane from the adjacent vehicular lane.

### PROTECTED BIKE LANES

A protected bike lane combines the user experience of a separated path with the on-street infrastructure of a conventional striped bike lane. It is physically separated from motor traffic and distinct from the sidewalk. These bike lanes are preferred, and should be used on key local streets within the study area.

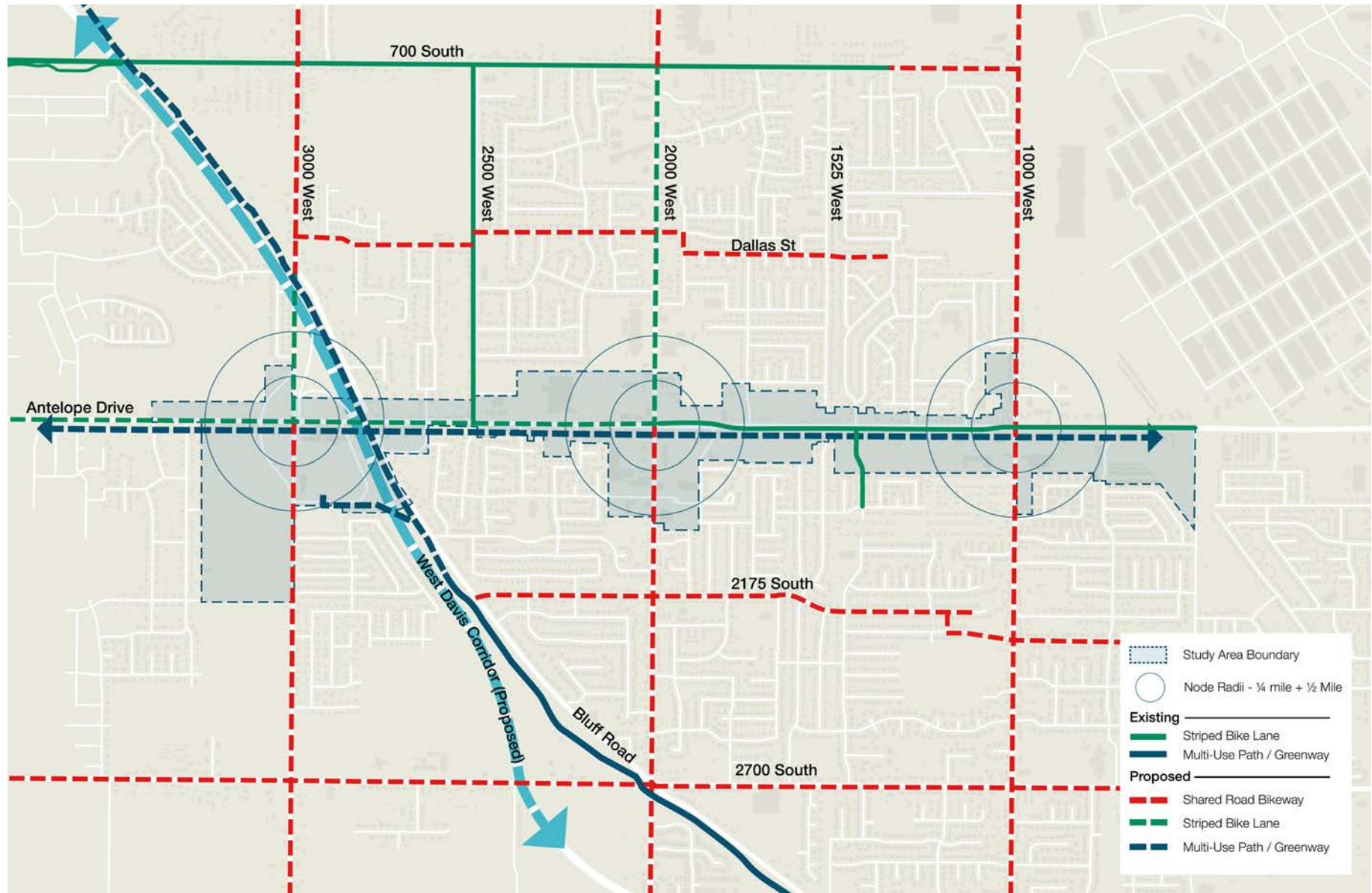
### MULTI-USE PATHWAYS + GREENWAY

The Antelope Greenway will provide an off-street protected bikeway through the entire study area, which provides a safe and comfortable experience for cyclists of all abilities. The trail connections also provide a cycling experience protected from vehicles.

*Some bike infrastructure information taken from the National Association of City Transportation (NACTO).*







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## Traffic Calming

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### OVERVIEW

Traffic calming measures are used to slow down traffic and increase awareness of drivers, creating a safer environment for pedestrians and cyclists, without reducing vehicle capacity.

### BULB OUTS

Bulb outs are used on streets with on-street parking to narrow the overall width of the street at intersections. They create shorter crossings for pedestrians and slow down vehicles.

### ON-STREET PARKING

On-street parking reduces the need for businesses to provide their own off-street parking. It also forces traffic to slow down when vehicles are pulling in and out of parking spaces.

### STREET TREES

Research has shown that street trees offer a psychological prompt for drivers to go slower.

### STREET LANE WIDTHS

Tighter lane widths are less forgiving than wide lanes, and therefore, drivers tend to slow down and become more aware of their surroundings.





# Transit

## OVERVIEW

The Utah Transit Authority (UTA) provides and operates local bus service for the Syracuse City.

Currently, there is only one bus route (Route 626) that services Syracuse City as illustrated in Figure 3-1. Within the project area, Route 626 travels along S 2000 West and Antelope Drive. Route 626 provides bus service between Clearfield Station and the City of Roy. During weekday peak hours, operating headways for Route 626 are approximately 10 minutes.

## FUTURE TRANSIT OPPORTUNITIES

As development intensifies in the three nodes, as proposed in this plan, the need for better transit solutions will likely arise. The node concept anticipates this, and sets up the ability for Bus Rapid Transit, or other dedicated transit system, to efficiently and conveniently move large numbers of people.

Additionally, the Clearfield FrontRunner Station is less than 3 miles from the Town Center node. This is a significant asset that will connect Syracuse to the entire Wasatch Front through convenient public transportation.









# DESIGN GUIDELINES

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## Guidelines Overview

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### INTENT

The intent of the Design Guidelines Section is to establish a design theme, coherent urban design and a consistent look and feel for the Antelope Drive Corridor. These guidelines are intended to enhance the experience visitors and residents of Syracuse have, making the city a destination spot to live, work and play.

There are many reasons to love Syracuse--its beautiful physical setting, proximity to nature and open space, and inviting small-town atmosphere. Along with promoting a safe and healthy environment, new development should support these existing positive attributes through capitalizing on inviting views when possible and maintaining human-scale development practices.

These guidelines create a design vocabulary that is unique to the Antelope Drive corridor in Syracuse. They promote a sense of aesthetic continuity, ensure high quality development, and help establish a clear and distinct community identity.

### DESIGN REVIEW COMMITTEE

A design review committee should be established to review all development within the study area to verify each project meets the vision outlined in this document and that all applicable design guidelines are followed.

### INTENT STATEMENT

The intent statement establishes the over-arching design intent for the category, and helps designers understand the rationale and aspiration used to create the design guidelines and standards. In the event the guidelines and standards are not clear or appropriate, the intent statement shall be referred to, in order to provide additional direction for the designers and the design review committee.

### DESIGN GUIDELINES

The design guidelines provide specific direction that designers should implement on their project. These guidelines provide important direction for designers and developers to ensure consistency across the various projects that will occur within the study area. It will also ensure that all participants in the development of the area will achieve a certain level of quality.

Guidelines use the term “should” or “may” to indicate that this direction should be implemented where possible and practical. In the event that a guideline is not applicable or appropriate, a process has been established to provide flexibility where necessary. The design review committee may grant exceptions if the applicant can clearly demonstrate a more appropriate solution that is still consistent with the intent, vision and guiding principles outlined in this document.





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## Site Layout Guidelines + Principles

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### INTENT

Strong urban design principles are characterized by thoughtfully choreographing buildings, landscape, open space and streets.

Simple changes to the layout and arrangement of buildings and parking on a site will have the most the most significant and positive impact in creating a walkable town center environment.

The appropriate arrangement of these elements reinforce the quality and functionality of the building facades, streets, and open spaces and how all of these elements work together to create a more livable environment.

### PRIMARY STREETS + FACADES

Primary streets should be identified in each node to establish a consistent streetwall with active ground floor uses. These will become the most important and walkable streets in each node. Buildings along primary streets should address the street with a primary facade and entrance.

Retail, residential, and/or other active uses are encouraged where a building faces a primary street.

### SECONDARY STREETS + FACADES

Secondary streets should be identified in each node. Buildings along secondary streets should still address the street with windows/transparency and high quality building materials, but are not as essential as primary streets. Retail, residential and/or other active uses are encouraged. Blank walls should be limited as much as possible.

### PARKING

Parking areas should be located in the rear and to the sides of buildings.

Buildings should wrap and screen parking areas from street where possible / applicable.

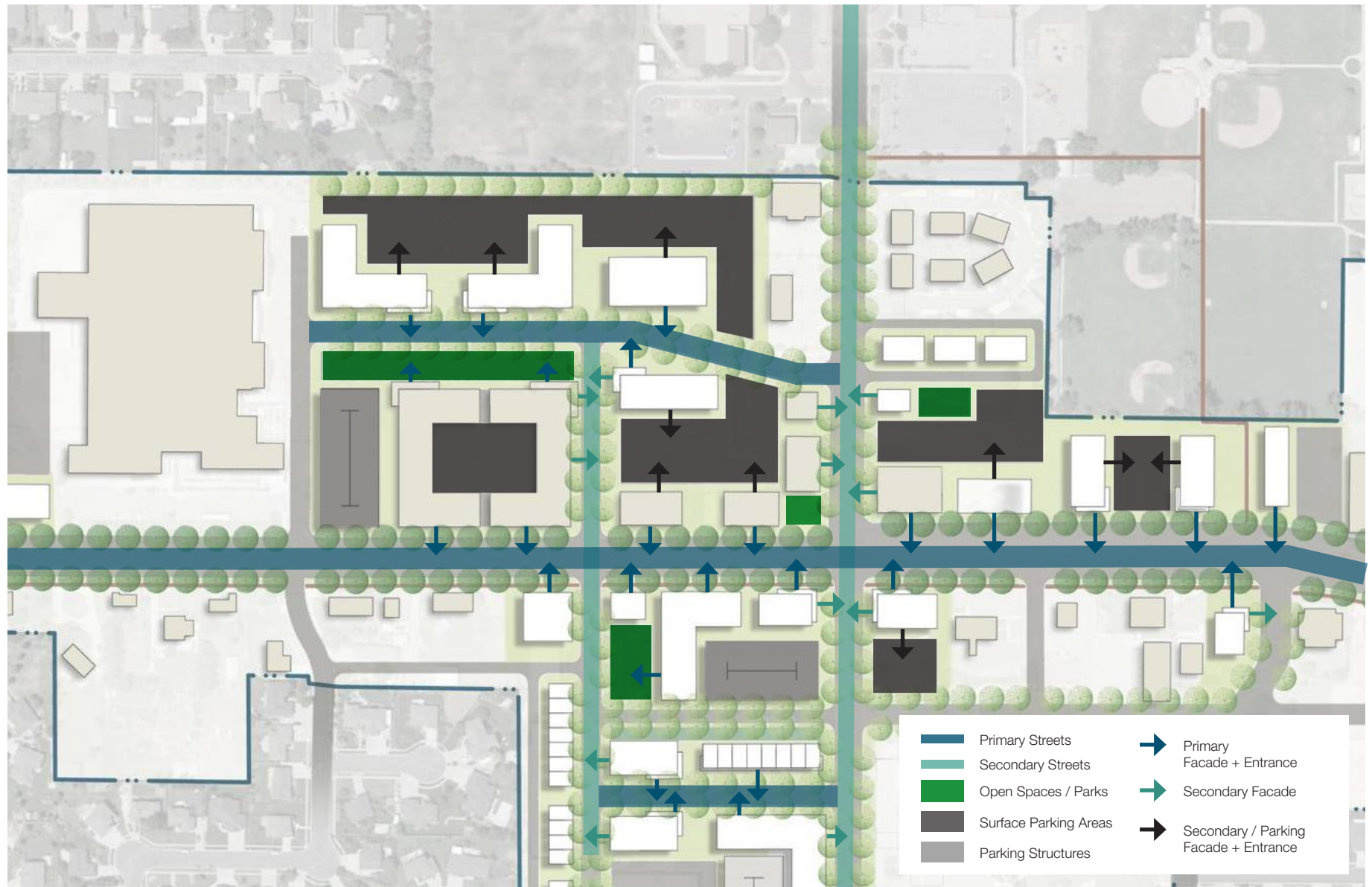
### OPEN SPACE

Open Space should be located throughout each node in various sizes and provide various user experiences.

Open spaces should be located in prominent areas.

Open space design and programming should respond to the surrounding uses and buildings.





## Build-to Line + Setbacks

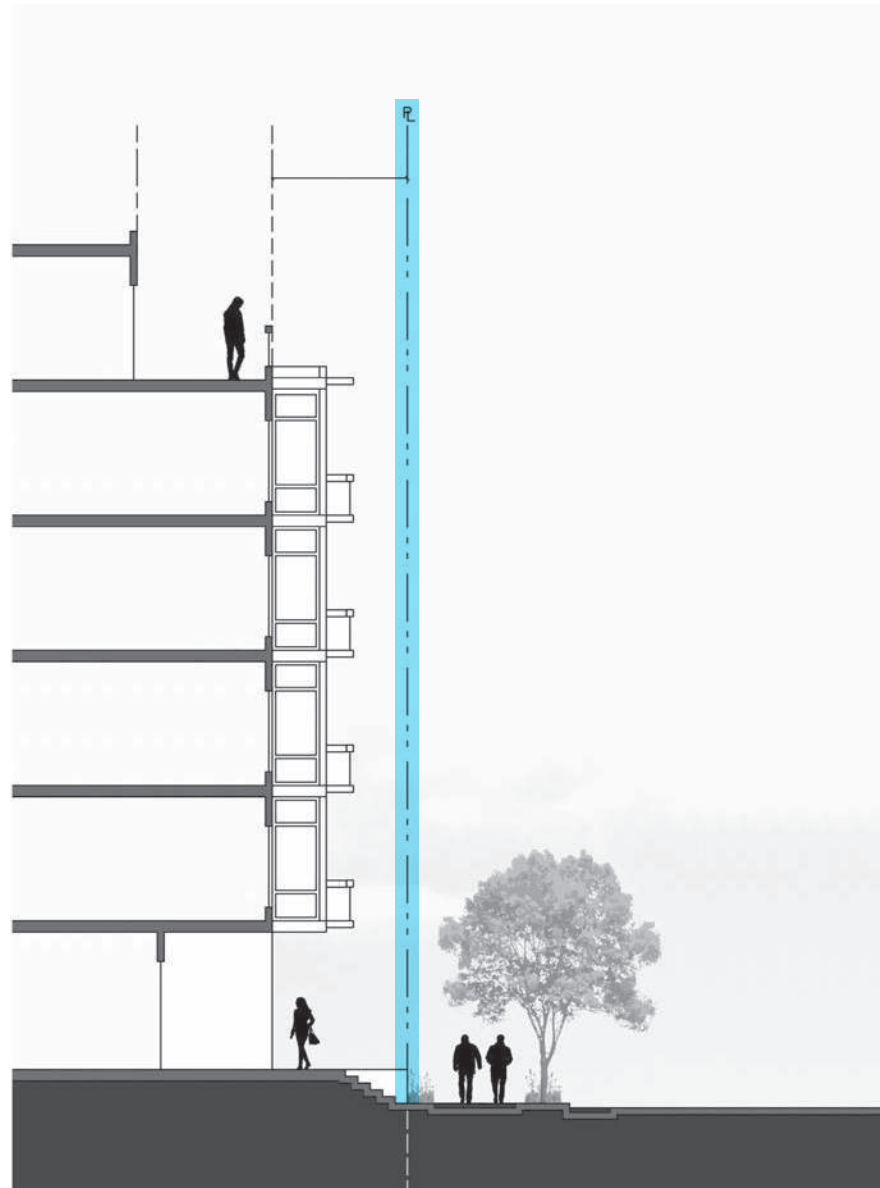
The build-to line refers to the line that distinguishes between the public and private realms. A building setback is the minimum required distance between the property line and the facade of a building.

### INTENT

To ensure all buildings consider their relationship with the public right-of-way and to create a defined streetwall.

### DESIGN GUIDELINES

- › Buildings should be situated at or close to the street to form a consistent and defined streetwall.
- › The setback distance will vary depending on the type of street that the building is fronting. Large arterials will have larger setbacks. Smaller local streets should have minimal to no setback.
- › The maximum setback allowed is 15 feet, except on Antelope Drive, where the maximum setback is 20'. See page 83 for details on Antelope Drive.





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## Land-Use Blending

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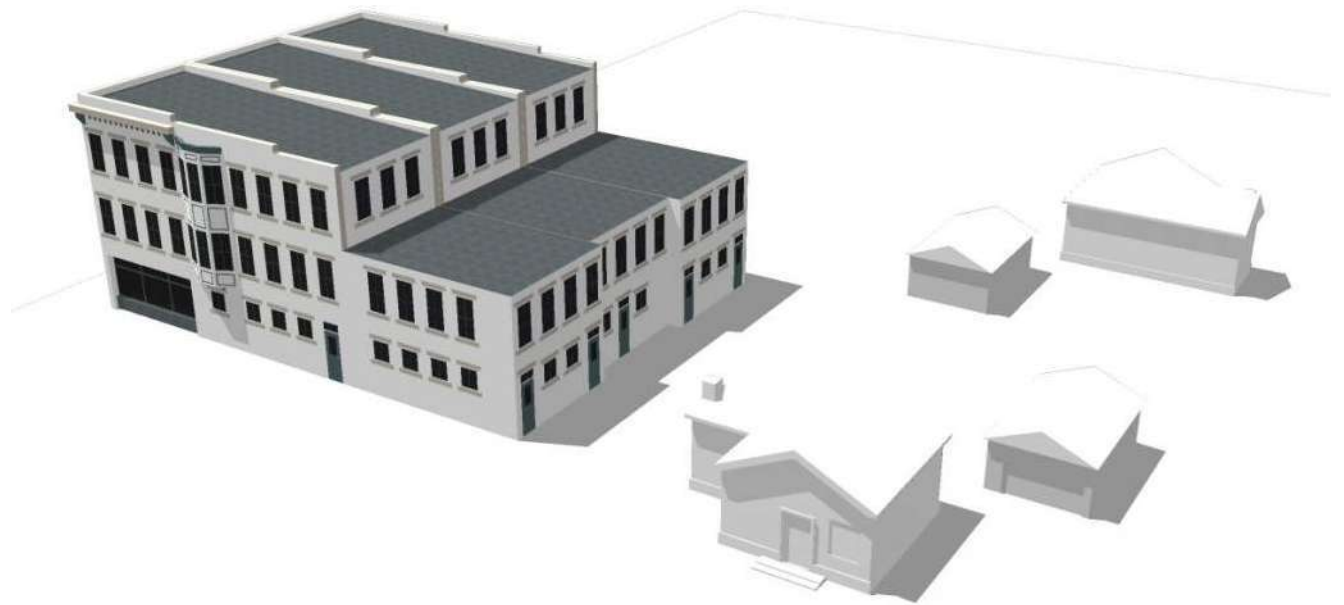
### INTENT

To ensure compatibility of new construction with the existing single family residences.

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### DESIGN GUIDELINES

- › For any building on a parcel adjacent to a single family detached home, buildings should:
- › Have a minimum 20 foot setback from shared parcel line.
- › Between 20 feet and 50 feet, building heights are limited to 30 feet high.



## Sign Standards (Standalone Signs)

### INTENT

To create a comprehensive and coordinated wayfinding system contributes to a place's ease and comfort for both visitors and residents, which provides both user groups a predictable experience, consistent image, and brand. It also supports commercial development by identifying businesses and directing visitors.

### DESIGN GUIDELINES

- › Signs should be artful, creative, add visual interest to the street, and complement the overall character of Syracuse City.
- › Signs should be utilized to direct vehicular and pedestrian traffic and be clearly identifiable.
- › Signs should incorporate and promote the brand for the City of Syracuse.

#### VEHICULAR SIGNS

- › Sign typeface should be clearly legible to individuals in moving vehicles. Signs should be oriented perpendicular to travel paths and are to be designed with high-quality, durable materials.
- › Externally lighted signs are encouraged, while backlit signs are discouraged.

#### PEDESTRIAN SIGNS

- › As with vehicular signs, pedestrian signs are also to have legible typeface and should be oriented perpendicular to travel paths.





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## Dark Skies + Lighting

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### INTENT

To minimize extraneous light and directs light only to areas and surfaces that should be illuminated. Dark Skies principles are especially important for Syracuse City because it is a major bird migratory pathway. It is also important for Syracuse to be consistent with Antelope Island State Park, which is a Dark Skies designated park.

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### DESIGN GUIDELINES

- › See the “Dark Sky Assessment Guide” and “Dark Sky Panning: Guidance & Best Practices” document provided through the Utah Workforce Services Housing & Community Development Office for more information.
- › Lighting should be designed to provide appropriate light levels and to avoid unnecessary spillover or glare.
- › Outdoor lighting performances should be based on both optics and overall system design, including distribution, functional and aesthetic requirements.
- › Lighting should be designed and located for specific tasks, including illumination of paths, entryways, parking, streets, and common areas.
- › Fixtures and poles/posts should be consistent throughout the study area. This will help contribute to sense of place and area identity.



## Architectural Style

### INTENT

To establish a specific “look and feel” throughout the study area to unify the area and create a design theme that is appropriate for Syracuse.

### DESIGN THEME

The design theme should be based on traditional architecture found in Northern Utah. The intent of the design theme is to “blend in”, rather than “stand out.”

This look will be established primarily through the design of the following elements:

- › Building massing
- › Roof lines
- › Proportion of windows/glazing
- › Building Materials

Each of these elements are detailed further in the design guidelines section.

While the overall character should emanate a traditional look and feel, contemporary elements may be blended in to highlight certain features and provide a fresh and updated look on a classic building.

The photos on this spread contain images that reflect the character envisioned in this document.









## Architectural Massing

### INTENT

To avoid visual bulk and monotony through appropriate massing and arrangement of buildings.

### DESIGN GUIDELINES

- › Building larger than two stories should have a clearly articulated top, middle, and bottom.
- › The form and materials of the base should firmly anchor the building to the site.
- › The form and materials of the top should differentiate it from the rest of the building, by lightening the look and mass of the building.
- › A covered walkway or arcade may be integrated into the base of a building to increase the visual aspect as a “base,” as well to provide protection from the elements for pedestrians.
- › Recesses and projections should be used to divide horizontal planes of the buildings into smaller-scale elements to create shadows, definition, and interest.
- › Large projects/developments should be broken up into multiple buildings of various sizes and heights.





## Facade Articulation

### INTENT

To promote detail and articulation in the facades of buildings in order to add architectural interest and richness, and limit long, monotonous facades.

### DESIGN GUIDELINES

- › The facade and mass of the building should be made clear to distinguish between various building functions, such as lobby, residential, retail, etc.
- › Building Facades facing streets should have an apparent width of no more than 100 feet without a break in massing, texture, color, or another element to break up the facade.
- › Facades should be modulated both vertically and horizontally to increase architectural interest
- › Buildings should use massing and facade articulation to minimize the apparent scale of buildings to feel more compatible with its context.





## Base Activation

### INTENT

To promote active uses of the street through ground floor building design elements.

### DESIGN GUIDELINES

- › Building walls facing streets should minimize blank walls.
- › Commercial buildings facades that face streets should include elements that encourage activity and interest, including transparency, operable windows, awnings, and display of goods or products.
- › Commercial buildings should have a ground floor ceiling height of at least 14 feet.
- › Outdoor dining is encouraged adjacent to restaurants.
- › Residential buildings should have entrances on facades facing the street.
- › Residential buildings may utilize a small setback between the sidewalk and buildings face for landscaping and/or stoops to increase privacy.
- › A covered walkway or arcade may be integrated into the base of a building to increase the visual aspect as a “base.”





## Attached Building Signage

### INTENT

Buildings may have signs to identify the commercial or non-commercial uses within the building. All signs must be approved by Design Review Committee.

- › All signs should be scaled appropriately to the size of the building.
- › Signs shall be constructed of high quality and durable materials that are consistent with and complement the building materials.
- › Building identification signage should be placed on facades that face the primary open space.
- › Signs should be artful and creative and add interest to a building's architecture.

### RESTRICTIONS

Internally illuminated box signs with more than 30% of the internal area illuminated are not permitted.

Animated, blinking, or flashing signs are not permitted.

### ACCEPTABLE SIGN TYPES

The following sign types are acceptable for attached building signs:

- 1 Wall signs** - Wall signs include signs that are attached to the face of a building wall. They should be mounted on the wall facing the public realm.
- 2 Window Signs** - Window signs are painted, placed, or affixed in or on the interior of a window, and intended to be viewed from the outside. Window signs should not obscure views into store or business.
- 3 Projecting Signs + Hanging Signs** - Projecting signs are attached to the building face and project out perpendicular to the building. Hanging signs are similar to projecting signs, except that they are suspended from a marquee or other overhead canopy.
- 4 Awning Signs** - Awning signs are signs that are mounted, printed on, painted on, or otherwise attached to an awning or canopy above a business door or window.
- 5 Mural** - Sign that is painted onto a wall that is visible to the public realm.



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## Materials Palette

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### INTENT

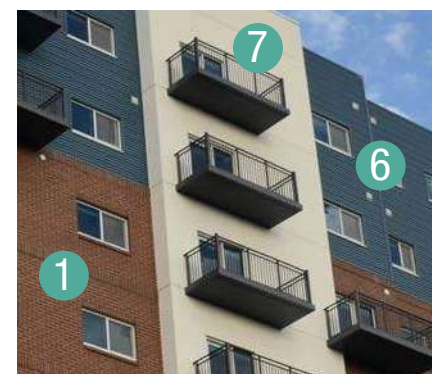
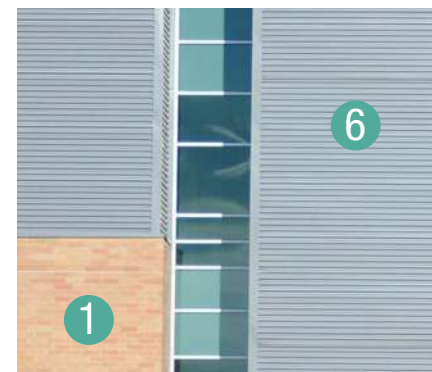
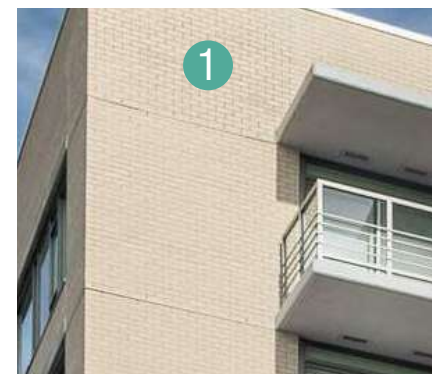
To ensure a consistent application of complementary and high quality materials throughout the study area that will reinforce the identity and sense of place.

### DESIGN GUIDELINES

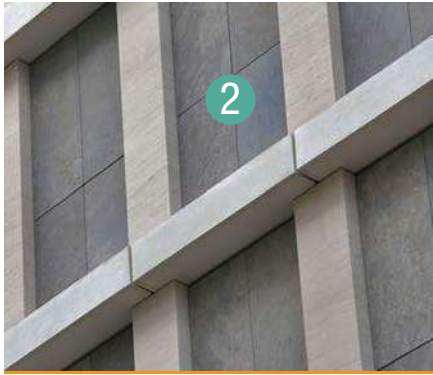
- › Building materials should be durable, high quality, and authentic materials that have a long life, age well, and reflect a high level of craftsmanship.
- › Building materials should add texture, depth, and visual interest to the building's facade.
- › Materials should turn corners and incorporate thoughtful transitions between facades, spaces, uses, and structures.
- › Materials should generally be limited to one or two predominant materials and one or two accent materials in order to keep buildings visually coherent and uncluttered.
- › EIFS Stucco should comprise no greater than 30% of the building's facade.
- › Wherever possible, use locally manufactured and/or recycled materials.

### ACCEPTABLE MATERIALS :

- 1 Brick
- 2 Natural Stone / Rock
- 3 Concrete
- 4 Cor-Ten Steel
- 5 Metal / Steel
- 6 Colored Pre-Finished Metal Panels
- 7 EIFS Stucco
- 8 Glass Fiber Reinforced Concrete
- 9 Glass / Glazing
- 10 High Quality Synthetic Stone







5

## Roofs

### INTENT

To emphasize the architectural style and to minimize visual impacts.

### DESIGN GUIDELINES

- › Pitched roofs are acceptable and appropriate and can be used to accentuate the traditional architectural style.
- › Flat roofs are acceptable if the overall style is consistent with a traditional “look and feel.”
- › Building heights and roof lines should modulate to create a visually appealing skyline and add interest to the skyline.
- › Mechanical equipment on roofs should be screened from the street view.
- › Green roofs are encouraged
- › Usable roof terraces are encouraged



## Corners

### INTENT

To emphasize important intersections and corners by including special architectural features on buildings that are located in these key locations.

### DESIGN GUIDELINES

- › Incorporate special design details and architectural treatments that reinforce the corner's importance as a public realm element
- › Corners in key locations should be emphasized by utilizing a combination of these measures:
  - A change in the building's massing and/or height
  - A contrasting facade finish
  - Transparency
- › Designers/Architects are encouraged to find creative and artful solutions.





## Entrances

### INTENT

To emphasize the relationship between buildings and their adjacent streets.

### DESIGN GUIDELINES

- › The main entrance to the building should provide the most important interaction between the pedestrian and building and should be emphasized through design.
- › Buildings that front primary streets should have a main entrance facing that street. A building may have an additional main entrance that faces the main parking area or drop-off zone, if applicable.
- › Use lighting to highlight entrances.



## Fenestration

### INTENT

To create pedestrian friendly and engaging relationship between buildings and streets.

### DESIGN GUIDELINES

- › The ground floor of commercial buildings should have a high percentage of transparent materials where buildings front streets.
- › Buildings maximize windows on upper floors that overlook streets or open spaces to increase “eyes on the street,” which discourages undesirable public behavior.
- › Windows should be strategically used next to entrances and open spaces to create prominent indoor/outdoor relationships.



## Streetscape

### INTENT

To create walkable, safe, and interesting streets that contribute to a liveable neighborhood and city.

### DESIGN GUIDELINES

All streetscapes within the study area should:

- › celebrate and create a draw to any nearby business entrances through paving detail and material
- › provide safe and intuitive pedestrian mobility between neighborhoods, commercial districts and greenways
- › include seating, and a comfortable resting environment at key locations
- › provide relief from the elements through the use of awnings, canopies, pergolas, and a consistent street tree canopy
- › establish a visually pleasing composition of trees, furniture, paving, lighting, and planting that reinforces community identity

The precedent images on this page show essential elements of successful commercial streetscapes.

### PRECEDENT IMAGES

- 1 Landscape strip separates walkway from street
- 2 Shade trees between street and pedestrian walkway
- 3 Awnings provide protection from elements and enhance entrances
- 4 Street activated by seating and dining
- 5 Ground level transparency
- 6 Building setback a short distance from to provide space for outdoor dining, planters, or other uses
- 7 Human scaled building signage identifies businesses and provides visual interest on streetscape
- 8 Street furnishings such as benches, trash receptacles, and street lights placed in landscape strip between street and walkway





## Antelope Drive Street Character

### INTENT

Antelope Drive is the most prominent, visible street in Syracuse. It runs through the heart of Syracuse and connects I-15 to Antelope Island. Because of this, Antelope Drive should have a specific “look and feel” that will help establish a brand and design character for the entire city.

### DESIGN GUIDELINES

The design for Antelope Drive should harken back to the City’s rural and rustic past, while also evoking the wildness of Antelope Island landscape. Elements that should be included in the design to reinforce this image include:

- › a split rail fence in the landscape strip
- › flowering perennials and grasses
- › Tall columnar trees in bulbouts that harken back to the rows of Lombardy Poplars that have traditionally lined fence lines along farms. Due to their short lifespan and general messiness, a substitute tree should be used in place of the Lombardy Poplar.

### DESIGN CONCEPT

- 1 Split Rail fence runs through the landscape strip
- 2 Flowering perennials and grasses evoke the rural Utah landscape
- 3 Street Trees provide shade and structure, as well as reduce traffic noise and pollution
- 4 Bulb-outs define space for parallel parking and shorten pedestrian crossings
- 5 Large columnar trees in bulb-outs recall the feeling of poplars lining farm fences, while also providing a strong structure for the street
- 6 Precedent image shows split rail fence in a field, which provides the basis for the design concept



## Arterials + Collectors (Antelope Drive)

### DEFINITION

Arterial and collector streets, defined in this document as any large, high capacity street. Traditionally, these streets are principally about vehicular mobility, often with at least four travel lanes and high speed limits. The principle arterials and collectors in the study area are Antelope Drive, 2000 West, 1000, West, and 3000 West.

### INTENT

High capacity arterials and collectors are essential for moving traffic in an effective manner throughout the City. However, too often these are unnecessarily designed ONLY for vehicular traffic. These streets should be designed to be safe, comfortable, and convenient for multiple travel modes, and each street should carefully consider how to balance the needs of all travel modes.

### DESIGN GUIDELINES

- › Lane widths should be as narrow as possible to reduce speeding
- › Traffic calming measures that don't reduce capacity should be implemented
- › Dedicated, safe bike facilities should be included where space is available
- › Safe, signaled street crossings should be spaced no more than 1/4 mile apart, and ideally much closer

### ANTELOPE DRIVE CONCEPT

The street section below shows a concept design for Antelope Drive. This concept uses setbacks to the existing right-of-way to create space for a multi-use pathway/greenway, as well as a generous landscape strip, with the ability to grow large street trees.





## EXAMPLE - ANTELOPE DRIVE

- 1 Large street trees planted in landscape strip
- 2 Multi-use path on both sides of street provides safe, family-friendly mobility options.
- 3 Painted bike lanes in the street provides an additional option for bicyclists.
- 4 Bulb-outs define space for parallel parking and shorten pedestrian crossings
- 5 Mid-block street crossing with dedicated pedestrian signal provides pedestrians with the ability to cross the street safely
- 6 Buildings are built to the edge of sidewalk, or with a small landscape setback to create a strong streetwall
- 7 Parking pockets provided by bulb-outs that discourage large truck parking while also providing traffic calming



## Local Street (Retail)

### DEFINITION

The Concept Development Plan encourages the creation of many new streets in each node, and each street should be designed to respond to its specific context. These new streets are intended to be lower-capacity local streets.

### INTENT

This page shows an example of a local retail street that demonstrates design how street layout and streetscape design should respond to the land-uses that line the street. In this case, mixed-use buildings with ground floor retail are the uses shown.

### DESIGN GUIDELINES

- › Lane widths should be as narrow as standards allow, with minimal lanes (typically no more than two travel lanes)
- › On-street parking should be used to provide closer access to businesses and residences, as well as acting as a traffic calming measure
- › Unique paving patterns and materials should be used to increase visual interest and enhance streetscape
- › Large street trees should be planted at close, consistent intervals (typically at least 30 feet on center)
- › Street furniture such as benches, trash receptacles, lighting and other amenities should be provided





## EXAMPLE - RETAIL STREET

- 1 Pedestrian friendly, narrow streets with only two lanes provide access to the businesses
- 2 Angled parking maximizes the amount of parking that can be accommodated on the street to improve access between parking and businesses
- 3 Unique paving provides visual interest and enhances streetscape
- 4 Large street trees provide a consistent canopy for shade and comfort
- 5 Crosswalks are clearly marked and located at all intersections
- 6 Buildings are built to the edge of sidewalk, or with a small landscape setback to create a strong streetwall



## Local Street (Office)

### DEFINITION

The Concept Development Plan encourages the creation of many new streets in each node, and each street should be designed to respond to its specific context. These new streets are intended to be lower-capacity local streets.

### INTENT

This page shows an example of a local street lined with office buildings that demonstrates design how street layout and streetscape design should respond to the land-uses that line the street. In this case, mixed-use buildings with ground floor retail are the uses shown.

### DESIGN GUIDELINES

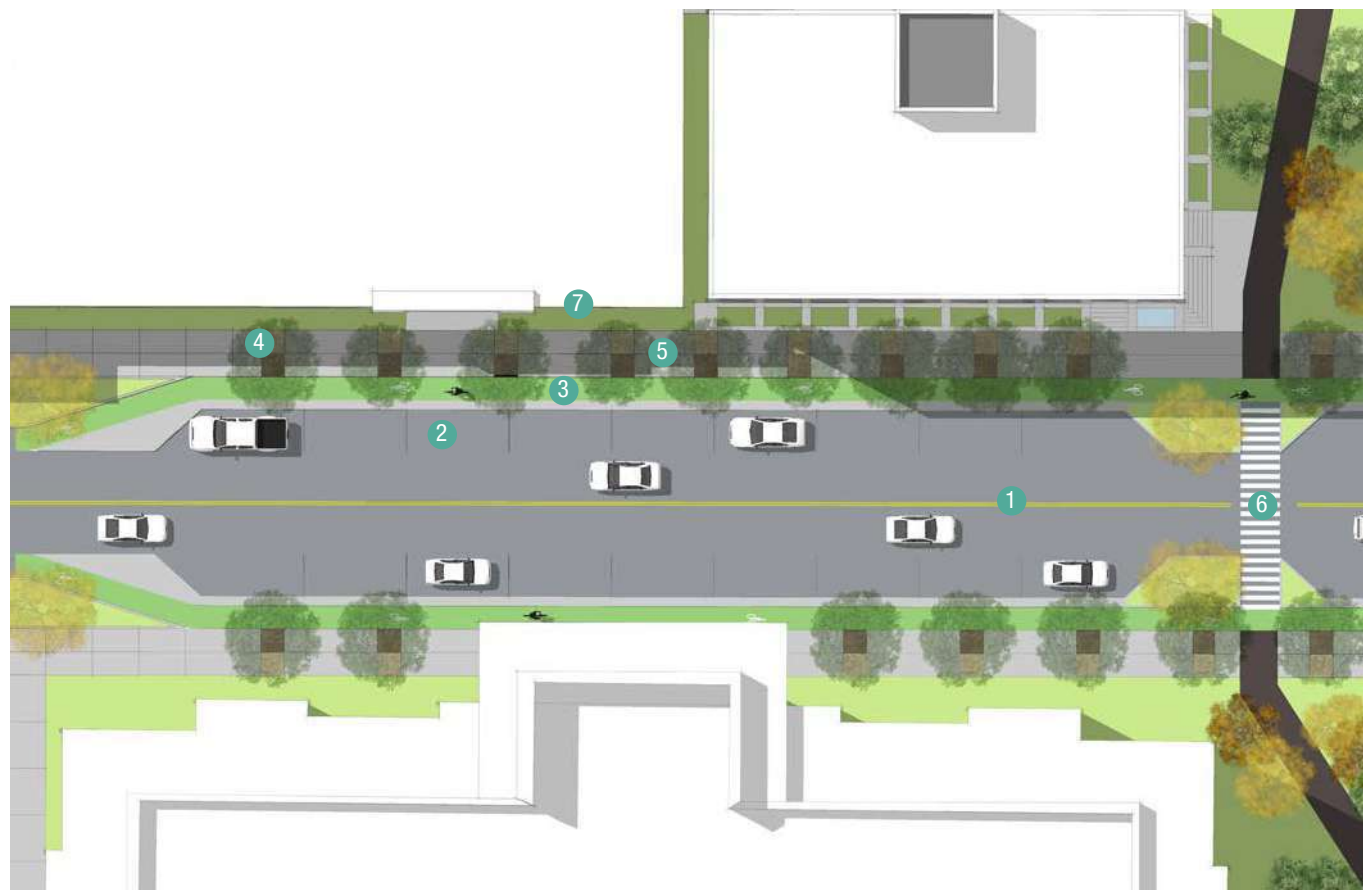
- › Lane widths should be as narrow as standards allow, with minimal lanes (typically no more than two travel lanes)
- › On-street parking should be used to provide closer access to businesses and residences, as well as acting as a traffic calming measure
- › Unique paving patterns and materials should be used to increase visual interest and enhance streetscape
- › Large street trees should be planted at close, consistent intervals (typically at least 30 feet on center)
- › Street furniture such as benches, trash receptacles, lighting and other amenities should be provided





## EXAMPLE - RETAIL STREET

- 1 Pedestrian friendly, narrow streets with only two lanes provide access to the businesses
- 2 Parallel parking provides convenient on-street parking for businesses and helps with traffic calming
- 3 A separated bike lane/cycle track provides safe and convenient bike infrastructure for bicyclists
- 4 Unique paving provides visual interest and enhances streetscape
- 5 Large street trees provide a consistent canopy for shade and comfort
- 6 Crosswalks are clearly marked and located at all intersections
- 7 Buildings are built to the edge of sidewalk, or with a small landscape setback to create a strong streetwall



## Planting Design

The landscape design is intended to be representative of the high desert Utah landscape. This should be done through the planting of grasses and shrubs that recall the native landscape. This should be supplemented with colorful perennials and other plants to add a level of refinement and designed beauty.

Planting choices should err on the side of natural and organic, rather than manicured and structured.

Water-wise landscaping is encouraged, particularly through the use of native plants. Lawn/turf grass should be used sparingly and strategically to reduce water use and enhance the natural planting look throughout the Antelope Drive Corridor.

### PHOTOS:

- 1 Native landscape and plant material on Antelope Island
- 2 Natural grasses and perennials are used in an urban streetscape, creating a unique “look and feel” that feels connected to the place.
- 3 Turf grass is used sparingly and creates interesting contrast
- 4 Ornamental grasses and colorful perennials create a distinct “sense of place”









## Plant Recommendations

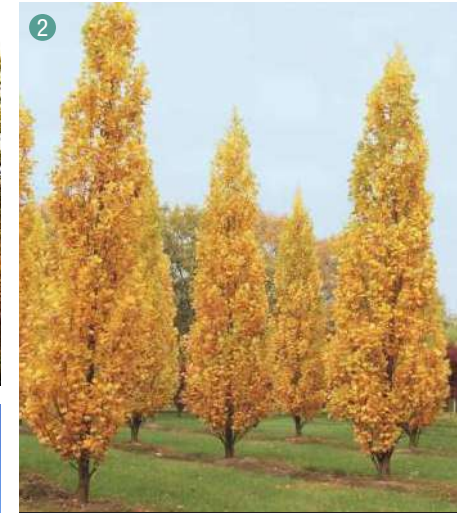
The following plant recommendations are examples of plant species that are compatible with the design vision. These plants are just a small sample of possibilities, but demonstrate the intended look and feel for the landscape.

### DECIDUOUS TREES

Use trees to improve pedestrian comfort along streets and trails and in parks, plazas, and other open spaces.

### SPECIES RECOMMENDATIONS

- 1 Norwegian Sunset Maple; *Acer truncatum* x *A. platanoides* 'Keithsform'
- 2 Swedish Aspen, *Populus tremula* 'Erecta'
- 3 Burr Oak; *Quercus macrocarpa*
- 4 Silver Linden; *Tilia tomentosa*
- 5 Goldenrain Tree; *Koelreuteria paniculata*
- 6 Accolade Elm; *Ulmus* x 'Accolade'
  - › London Plane Tree; *Platanus* x *Acerifolia*
  - › Western Catalpa; *Catalpa speciosa*





## CONIFEROUS TREES

Coniferous trees are recommended to screen views into adjacent residential properties, or where loading, unloading, and service areas are located. They can also be used as an accent or highlight for special areas in the landscape.

## SPECIES RECOMMENDATIONS

- ① Blue Atlas Cedar; *Cedrus atlantica* 'Glauca'
- ② Deodar Cedar; *Cedrus deodard*
  - › White Fir; *Abies concolor*
- ③ Vanderwolf's Pine; *Pinus flexilis* 'Vanderwolf's Pyramid'
  - › Pinyon Pine; *Pinus edulis*
  - › Austrian Pine; *Pinus nigra*
- ④ Western Larch; *Larix occidentalis*
- ⑤ Blue Spruce; *Picea pungens*
- ⑥ Scotch Pine; *Pinus sylvestris*
  - › Norway Spruce; *Picea abies*



## SHRUBS + PERENNIALS

Shrubs and perennials are fillers and accents in the landscape. Group shrubs and perennials in clusters that range in size for naturalistic plant massing patterns.. Random scattering of individual plants is discouraged. Consider year round color and interest when selecting plants. Increase focus on colors and interesting arrangements in key areas of the landscape.

## SPECIES RECOMMENDATIONS

### PERENNIALS

- 1 Blue Mist; *Caryopteris x. clandonensis*
  - › Penstemon species
  - › Globemallow; *Spharealcea ssp*
  - › Artemisia; *Arctostaphylos uva ursi*
- 2 Yarrow; *Achillea millefolium*
  - › Mexican Blue Sage; *Salvia chamaedryoides*
- 3 Daylily; *Hemerocallis hybrid*
  - › Black Eyed Susan; *Rudbeckia fulgida*
  - › Russian Sage; *Perovskia atriplicifolia*
- 4 Woodland Sage; *Salvia nemorosa*
- 5 Coneflower; *Echinacea varieties*

### DECIDUOUS SHRUBS

- 6 Dwarf Smooth Sumac; *Rhus glabra v cistmontana*
  - › Peking Cotoneaster; *Cotoneaster acutifolia*
- 7 Golden Currant; *Ribes Aureum*
  - › Rose Daphne; *Daphne cneorum*
  - › Leatherleaf Viburnum; *Viburnum*
  - › Red Chokeberry; *Aronia arbutifolia*
  - › Mentor Barberry, Red Leaf Barberry, Rose Glow Barberry; *Berberis sp.*
  - › Butterfly Bush; *Buddleia sp.*
  - › Thinleaf Alder; *Alnus incana; Alnus tenuifolia*
- 8 Winged Euonymous; *Euonymous alata*

- › Kinnikinnick; *Arctostaphylos uva ursi*
- › Diablo Ninebark; *Physocarpus opulifolius 'Monlo'*
- 9 Red Twig Dogwood; *Cornus alba*
  - › Lavender; *Lavandula angustifolia*
  - › Serviceberry; *Amelanchier utahensis*
  - › English Privet; *Ligustrum vulgare 'Lodense'*
- 10 Gro-Low Sumac; *Rhus aromatic 'Gro-Low'*

### EVERGREEN SHRUBS

- › Mountain Lover; *Paxistima myrsinites*
- › Juniper; *Juniperus ssp.*
- 11 Mugo Pine; *Pinus mugo var. mugo*
  - › Dense Yew; *Taxus x media 'Densiformis'*
  - › Curl-leaf Mountain Mahogany; *Cercocarpus ledifolius*
- 12 Oregon Grape; *Mahonia repens*







## GRASSES + SUCCULENTS

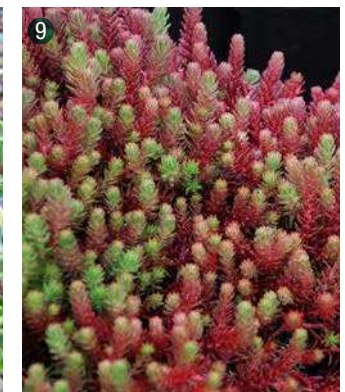
Grasses and succulents can make powerful visual statements in the landscape. Both types of plants serve as effective space fillers and attractive accents to shrubs and perennials. Groupings of grasses can create a softer, more welcoming feel for a space and the proper succulent can be a striking specimen in the landscape, whether as an individual or in a group.

### GRASSES

- 1 Blue Fescue; *Festuca ovina glauca*
- 2 Blue Grama; *Bouteloua gracilis*
- 3 Dwarf Fountain Grass; *Pennisetum alopecuroides 'Hameln'*
- › Feather Reed Grass; *Calamagrostis Karl Foerster*
- 4 Switchgrass; *Panicum virgatum*
- 5 Blue Oat Grass; *Helictotrichon sempervirens*
- 6 Great Basin Rye; *Elymus cinereus*

### SUCCULENTS

- 7 Sedum; *Sedum 'Pool Party'*
- 8 Variegated Russian Stonecrop; *Sedum kamtschaticum 'Variegatum'*
- 9 Stonecrop; *Sedum ochroleucum 'Red Wiggle'*





## VINES + GROUNDCOVERS

Vines and groundcovers work great for filling in large, bare areas. A concrete wall or a field of gravel can enhance an otherwise bland area.

### VINES

- 1 Cat's Claw Vine; *Macfadyena unguis-Cacti*
  - › Flowering Jasmine; *Jasminum floridum*
  - › Lady Banks' Rose; *Rosa banksiae*, 'Alba Plena' or 'Lutea'
- 2 Star Jasmine; *Trachelospermum jasminoides*
- 3 Trumpet Vine; *Campsis radicans*

### GROUNDCOVERS

- 4 Angelita Daisy; *Hymenoxys acaulis*
  - › Blackfoot Daisy; *Melampodium leucanthum*
  - › Bursage; *Amobrosia deltoidea*
  - › California Fuchsia; *Zauschneria californica*
  - › Carpet Rose; *Rosa Carpet*
  - › Daylily; *Hemerocallis hybrid*
- 5 Creeping Juniper; *Juniperus horizontalis*
  - › Desert Milkweed; *Asclepias subulata*
  - › Dwarf Lantana; *Lantana varieties*
  - › Gazania; *Gazania species*
  - › Globemallow; *Sphaeraicea ambigua*
  - › Golden Columbine; *Aquilegia chrysantha*
  - › Grey/Green Santolina; *Santolina varieties*
- 6 Ice Plant; *Desloperma varieties*

- 7 Penstemon; *Penstemon species*
  - › Dalea; *Dalea capitata* 'Sierra Gold' or 'Sierra Negra'
  - › Verbena; *Verbena species*
  - › Zinnia; *Zinnia grandiflora*













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# PLANNING COMMISSION WORK SESSION

January 15, 2019

## Agenda Item # 2b

## §10.35.040 Home Occupation Parking Standards

### *Factual Summation*

Please review the following information. Any questions regarding this agenda item may be directed to Royce Davies, City Planner or Noah Steele, CED Director.

### *Summary*

The City Council has directed staff to address the parking standards in the home occupation business section of the City Code to regulate parking for large vehicles and trailers and those with signage. A resident with a business on 2700 South requested that the City Council address these regulations.

In response, staff has written a potential amendment to subsection (B)(7) of §10.35.040 which has been included in this report. The intent of the amendment is to allow the parking of vehicles which may have been previously prohibited due to size, signage, or the number of vehicles on properties that front busy sections of road. Properties must also be large enough to accommodate parking in the side or rear yards and have adequate screening. Otherwise, the standard limitations on parking vehicles for home occupation businesses applies.

### *Attachments:*

- Existing §10.35.040(B)(7)
- Proposed §10.35.040(B)(7)

**EXISTING §10.35.040(B)(7)**

(7) Residents may have one occupational vehicle and trailer, associated with the home occupation, that does not exceed 10,000 pounds gross vehicle weight for minor home occupations and 20,000 pounds gross vehicle weight for major home occupations, on site, provided they park said vehicles off the street, in compliance with residential off-street parking standards, and not on any street adjacent to or near their premises. By way of illustration and not limitation, this subsection prohibits the storage or parking of business vehicles, such as limousines, service or work vehicles (snowplows, landscape and maintenance trucks, etc.), and similar vehicles as well as delivery and contractor's vehicles, equipment, and trailers used to transport same. Business owners may not intentionally station, position, or park an occupational vehicle or trailer in any manner on private property so as to advertise, promote, or draw attention to products, services, events, or other similar purposes at the homeowner's residence while parked thereon.



## **PROPOSED §10.35.040(B)(7)**

### **(7) Home Occupation Parking Standards**

(a) One home occupation vehicle and trailer may be stored off-street only on the home occupation property according to the following:

- (i) Minor home occupations are permitted vehicles up to 10,000 pounds gross vehicle weight.
- (ii) Major home occupations are permitted vehicles up to 20,000 pounds gross vehicle weight.

(b) Additional home occupation vehicles and trailers may be permitted on home occupation properties with the following attributes:

- (i) Fronting State Highway 193 between 1000 West and 3000 West; or
- (ii) Fronting Antelope Drive (1700 South) between 1000 West and 3000 West; or
- (iii) Fronting Gordon Avenue (2700 South) between 1000 West and 2000 West; or
- (iv) Fronting 2000 West between Antelope Drive (1700 South) and State Highway 193;
- (v) Consisting of more than 0.5 acres; and
- (vi) Having a view-obscuring fence or wall including view-obscuring gate(s) surrounding the parking area; and
- (vii) Having one tree or shrub at least 10 feet in height around any perimeter of the parking area that faces a public street; and
- (viii) Having all home occupation parking paved with concrete or asphalt; and
- (ix) Restricting the parking area to the side or rear yards of the home; or
- (x) Having enclosed parking and/or storage space. Enclosed parking shall not utilize required parking for a dwelling unless the home occupation vehicle is also a primary use vehicle.

(c) Any vehicles with business signage must be parked in accordance with subsection (b) unless the signage is a total of 4 square feet or less.

(d) The following may only be parked or stored in accordance with subsection (b):

- (i) Limousines
- (ii) Service or heavy work vehicles (e.g. snowplows, landscape and maintenance trucks)
- (iii) Delivery vehicles
- (iv) contractor's vehicles
- (v) Trailers
- (vi) Equipment